

ARTICLE 8: Parking and Loading Standards	8-1
17.8.1: General	8-1
17.8.1.1: Purpose and Intent	8-1
17.8.1.2: Applicability	8-1
17.8.1.3: General Standards	8-1
17.8.1.4: Design and Development Manuals	8-2
17.8.2: Visitor Parking	8-9
17.8.2.1: General Standards	8-9
17.8.3: Parking Substitutions and Reductions	8-9
17.8.3.1: General Standards	8-9
17.8.4: Bicycle Parking	8-10
17.8.4.1: General Standards	8-10
17.8.4.2: Long-term Bicycle Parking.....	8-11
17.8.5: Electric Vehicle Parking Standards	8-12
17.8.5.1: General Standards	8-12
17.8.5.2: Electric Vehicle Charging Station Standards	8-13
17.8.6: Single-family, Duplex and Attached Residential Parking Standards.....	8-14
17.8.6.1: Driveways and Parking Areas.....	8-14
17.8.7: Multifamily, Institutional, Mixed-Use, Commercial, and Light Industrial Parking and Loading Standards	8-15
17.8.7.1: Americans with Disabilities Act Parking Standards.....	8-15
17.8.7.2: Loading Space Standards	8-15
17.8.7.3: Sight Triangles.....	8-16
17.8.7.4: Vehicle Stacking	8-16
17.8.8: Surface Parking Lot Dimensions	8-17
17.8.8.1: General Standards	8-17
17.8.9: Parking Lot Placement and Design	8-20
17.8.9.1: Parking Lot Location.....	8-20
17.8.9.2: Parking Blocks	8-20
17.8.10: Parking Lot Landscape Standards	8-22
17.8.10.1: Landscaping in Parking Lots	8-22
17.8.10.2: Screening of Parking Lots.....	8-24

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ARTICLE 8: PARKING AND LOADING STANDARDS

17.8.1: General

17.8.1.1: Purpose and Intent

This Article establishes parking and loading standards for development in the City of Lakewood. The purpose of these parking and loading standards is to ensure that development implements the principles and goals articulated in the Comprehensive Plan for land use, community sustainability and transportation. The purpose of the parking and loading standards in this Article is to:

- A. Provide for pedestrian connections and safety.
- B. Prevent the establishment of excessive amounts of off-street parking.
- C. Reduce the need for parking by promoting the use of transit, bicycles, and other alternative forms of transportation.

17.8.1.2: Applicability

This Article establishes parking standards for all land uses in the City. The parking standards shall be applied for any addition or new construction except where explicitly superseded by an approved Official Development Plan or as identified in this Article.

Additions or new construction shall follow the site plan process outlined in Article 2 of this Zoning Ordinance. Provisions for nonconforming parking and loading standards shall be found in Article 12 of this Zoning Ordinance.

17.8.1.3: General Standards

- A. Provision of parking spaces within an integrated parking and access system is required. The total number of parking spaces provided shall be the sum total of the individual parking standards. Mixed developments, shopping centers, and industrial or office parks, shall be evaluated on individual uses, however shared parking agreements, and the relationship between specific uses shall be used to determine parking standards. Off-street parking standards are indicated in [Table 17.8.1](#)
- B. The minimum and maximum off-street vehicle and bicycle parking standards identified in [Table 17.8.1](#) shall apply to all new development and redevelopment.
- C. Parking standards that are based on building square footage, outdoor recreational field square footage, and/or outdoor entertainment area shall be calculated on the gross floor area of a building, field or entertainment area.
- D. For motor vehicles sales, the parking requirement will be calculated on the building footprint.

- E. When measurements of the number of required spaces result in a fractional number, any fraction of one-half or less will be rounded down to the next lower whole number and any fraction of more than one-half will be rounded up to the next higher whole number.
- F. Parking maximums shall not apply to structured parking.
- G. Driveways shall not count as a parking space(s).
- H. On-site parking shall be maintained in good condition free of weeds, dust, trash and debris, and major surfacing defects.
- I. No person shall construct, pave or repave a parking lot without first obtaining a building permit. Newly paved and repaved parking lots shall comply with Americans with Disabilities Act Parking Standards.

17.8.1.4: Design and Development Manuals

In addition to the parking standards established in this Article, the City has adopted several design and development manuals to further articulate the intended design for specific areas of the City. Any new development or redevelopment of a site located in an area governed by a design manual shall adhere to the standards and guidelines outlined in such manuals.

Parking Standards [Table 17.8.1](#) identifies the minimum and maximum parking requirements for all uses in all zone districts.

Table 17.8.1: Parking Standards						
Land Use	Vehicle Parking				Bicycle Parking	
	Minimum	Maximum			Long-Term	Short-Term
	All Districts	Residential, Commercial, Light Industrial and Suburban Context	Urban Context	Transit Context	All Districts	All Districts
Residential						
Single-Family dwelling unit	NA*	7 spaces per unit	3 spaces per unit	2 spaces per unit	NA	NA
Accessory dwelling unit	1 space per unit	2 spaces per unit	2 spaces per unit	2 spaces per unit	NA	NA
Duplex dwelling unit	NA	4 spaces per unit	2 spaces per unit	2 spaces per unit	NA	NA
Attached dwelling unit	NA	3 spaces per unit	2 spaces per unit	2 spaces per unit	NA	NA
Multifamily dwelling unit						
Residential, Commercial, Light Industrial and Suburban	1.5 spaces per unit	3 spaces per unit	NA	NA	1 space per 2 units	1 space per 10 units
Urban	1.25 spaces per unit	NA	2 spaces per unit	NA		
Transit	1 space per unit	NA	NA	1.5 spaces per unit		
Mobile Home	1 space per unit	2 spaces per unit	NA	NA	NA	NA
Group Home	2 spaces per unit	5 spaces per unit	4 spaces per unit	3 spaces per unit	NA	NA
Group Residential Facility	0.25 spaces per bedroom	2.0 spaces per bedroom	1.5 spaces per bedroom	1.0 space per bedroom	1 space per 25 bedrooms	1 space per 35 bedrooms
Shelter	0.25 spaces per 1,000 sf	1.5 space per 1,000 sf	0.75 spaces per 1,000 sf	0.50 spaces per 1,000 sf	1 space per 2,500 sf	1 space per 5,000 sf

Table 17.8.1: Parking Standards						
Land Use	Vehicle Parking				Bicycle Parking	
	Minimum	Maximum			Long-Term	Short-Term
	All Districts	Residential, Commercial, Light Industrial and Suburban Context	Urban Context	Transit Context	All Districts	All Districts
Commercial and Industrial						
Adult Business	1 space per 1,000 sf	3 space per 1,000 sf	2.5 spaces per 1,000 sf	2.0 space per 1,000 sf	1 space per 2,500 sf	1 space per 5,000 sf
Animal Care	2 spaces per 1,000 sf	4 spaces per 1,000 sf	3.5 space per 1,000 sf	3 spaces per unit	1 space per 2,500 sf	1 space per 5,000 sf
Bar	2 spaces per 1,000 sf	6 spaces per 1,000 sf	5 spaces per 1,000 sf	4 spaces per 1,000 sf	NA	1 space per 5,000 sf
Bed and Breakfast	1 space per bedroom	4 spaces plus 1 space per bedroom	3 spaces plus 1 space per bedroom	2 spaces plus 1 space per bedroom	NA	NA
Cemetery	NA	NA	NA	NA	NA	NA
Club, Lodge, or Service Organization	2 spaces per 1,000 sf	5 spaces per 1,000 sf	4 spaces per 1,000 sf	3 spaces per 1,000 sf	1 space per 2,500 sf	1 space per 5,000 sf
Contractor Shop	1 space per 1,000 sf	3 spaces per 1,000 sf	NA	NA	1 space per 2,500 sf	NA
Crematory	1 space per 1,000 sf	3 spaces per 1,000 sf	NA	NA	NA	NA
Day Care Facility, Child or Adult	1.5 spaces per 1,000 sf	4 space per 1,000 sf	3 space per 1,000 sf	2.5 space per 1,000 sf	1 space per 1,000 sf	1 space per 5,000 sf
Emergency Medical Facility	1 space per 1,000 sf	4 space per 1,000 sf	3 space per 1,000 sf	2 spaces per 1,000 sf	1 space per 2,000 sf	1 space per 5,000 sf
Entertainment Facility						
Indoor	2.5 spaces per 1,000 sf	5 spaces per 1,000 sf	4 spaces per 1,000 sf	3 spaces per 1,000 sf	1 space per 5,000 sf	1 space per 1,000 sf
Outdoor	2 space per 1,000 sf	6 spaces per 1,000 sf	5 spaces per 1,000 sf	4 spaces per 1,000 sf	NA	1 space per 5,000 sf
Fitness or Athletic Facility, Private	2 spaces per 1,000 sf	5 spaces per 1,000 sf	4 spaces per 1,000 sf	3 spaces per 1,000 sf	1 space per 2,500 sf	1 space per 1,000 sf

Table 17.8.1: Parking Standards						
Land Use	Vehicle Parking				Bicycle Parking	
	Minimum	Maximum			Long-Term	Short-Term
	All Districts	Residential, Commercial, Light Industrial and Suburban Context	Urban Context	Transit Context	All Districts	All Districts
Gallery or Studio	1 space per 1,000	4 per spaces 1,000 sf	3 spaces per 1,000 sf	2 spaces per 1,000 sf	1 space per 5,000 sf	1 space per 5,000 sf
Golf Course	2 spaces per 1,000 sf	5 spaces per 1,000 sf	4 spaces per 1,000 sf	3 spaces per 1,000 sf	1 space per 5,000 sf	1 space per 5,000 sf
Hotel	1 per 1,000 sf	4 per 1,000 sf	3 per 1,000 sf	2 per 1,000 sf	1 per 5,000 sf	1 space per 10,000 sf
Junkyard or Motor Vehicle Wrecking	0.25 per 1,000 sf	2 per 1,000 sf	NA	NA	NA	1 space per 10,000 sf
Manufacturing						
Light	1 per 1,000 sf	4 spaces per 1,000 sf	3 spaces per 1,000 sf	2 spaces per 1,000 sf	1 per 2,500 sf	NA
Heavy	1 per 1,000 sf	4 spaces per 1,000 sf	NA	NA	1 per 2,500 sf	NA
Medical Marijuana Business	1.5 per 1,000 sf	4 spaces per 1,000 sf	3 spaces per 1,000 sf	2 spaces per 1,000 sf	1 per 2,500 sf	1 space per 5,000 sf
Mini-Warehouse or Storage	0.1 per 1,000 sf	.2 per 1,000 sf	NA	NA	1 per 20,000 sf	NA
Mortuary	1 per 1,000 sf	4 spaces per 1,000 sf	3 spaces per 1,000 sf	2 spaces per 1,000 sf	1 per 2,500 sf	1 space per 5,000 sf
Motel	1 per 1,000 sf	4 per 1,000 sf	NA	NA	1 per 5,000 sf	1 space per 10,000 sf
Motor Vehicle Rental	1 per 1,000 sf	4 spaces per 1,000 sf	3 spaces per 1,000 sf	2 spaces per 1,000 sf	1 per 2,500 sf	1 space per 5,000 sf

Table 17.8.1: Parking Standards						
Land Use	Vehicle Parking				Bicycle Parking	
	Minimum	Maximum			Long-Term	Short-Term
	All Districts	Residential, Commercial, Light Industrial and Suburban Context	Urban Context	Transit Context	All Districts	All Districts
Motor Vehicle Sales	0.25 per 1,000 sf	3 spaces per 1,000 sf	2 spaces per 1,000 sf	NA	1 per 2,500 sf	1 space per 5,000 sf
Motor Vehicle Service						
Car Wash	0.25 per 1,000 sf	3 spaces per 1,000 sf	2 spaces per 1,000 sf	1 spaces per 1,000 sf	1 per 2,500 sf	1 space per 5,000 sf
Fueling Station	0.25 per 1,000 sf	3 spaces per 1,000 sf	2 spaces per 1,000 sf	1 spaces per 1,000 sf	1 per 2,500 sf	1 space per 5,000 sf
Major	0.25 per 1,000 sf	3 spaces per 1,000 sf	2 spaces per 1,000 sf	1 spaces per 1,000 sf	1 per 2,500 sf	1 space per 5,000 sf
Minor	0.25 per 1,000 sf	4 spaces per 1,000 sf	2 spaces per 1,000 sf	1 spaces per 1,000 sf	1 per 2,500 sf	1 space per 5,000 sf
Office	1.5 per 1,000 sf	5 spaces per 1,000 sf	3.5 spaces per 1,000 sf	2.5 spaces per 1,000 sf	1 per 2,000 sf	1 space per 5,000 sf
Parking, Stand-Alone	NA	NA	NA	NA	NA	1 space per 5,000 sf
Pawnbroker	1 per 1,000 sf	4 spaces per 1,000 sf	3 spaces per 1,000 sf	2 spaces per 1,000 sf	1 per 2,500 sf	1 space per 2,000 sf
Personal Service	1 per 1,000 sf	4 spaces per 1,000 sf	3 spaces per 1,000 sf	2 spaces per 1,000 sf	1 per 2,500 sf	1 space per 2,000 sf
Plant Nursery	1 space per 2,500 sf	4 spaces per 1,000 sf	NA	NA	1 space per 5,000 sf	1 space per 10,000 sf

Table 17.8.1: Parking Standards						
Land Use	Vehicle Parking				Bicycle Parking	
	Minimum	Maximum			Long-Term	Short-Term
	All Districts	Residential, Commercial, Light Industrial and Suburban Context	Urban Context	Transit Context	All Districts	All Districts
Restaurant						
Residential, Commercial, Light Industrial and Suburban	9 spaces per 1,000 sf	12 spaces per 1,000 sf	NA	NA	1 space per 2,000 sf	1 space per 2,000 sf
Urban	6 spaces per 1,000 sf	NA	8 spaces per 1,000 sf	NA		
Transit	4 spaces per 1,000 sf	NA	NA	5 spaces per 1,000 sf		
Retail	1 spaces per 1,000 sf	5 spaces per 1,000 sf	4 spaces per 1,000 sf	3 spaces per 1,000 sf	1 space per 2,500 sf	1 space per 2,000 sf
Rental, Service, or Repair of Large Items	1 space per 1,000 sf	5 spaces per 1,000 sf	NA	NA	1 space per 2,500 sf	1 space per 5,000 sf
Storage, Outdoor	NA	NA	NA	NA	NA	NA
Vehicle Dispatch Facility	NA	NA	NA	NA	NA	NA
Warehouse or Distribution	0.25 spaces per 1,000 sf	1 space per 1,000 sf	NA	NA	1 space per 5,000 sf	1 space per 10,000 sf
Public / Civic / Institutional						
Community Building	2 spaces per 1,000 sf	5 spaces per 1,000 sf	4 spaces per 1,000 sf	3 spaces per 1,000 sf	1 space per 2,500 sf	1 space per 5,000 sf

Table 17.8.1: Parking Standards						
Land Use	Vehicle Parking				Bicycle Parking	
	Minimum	Maximum			Long-Term	Short-Term
	All Districts	Residential, Commercial, Light Industrial and Suburban Context	Urban Context	Transit Context	All Districts	All Districts
Convention or Exposition Center	2 spaces per 1,000 sf	5 spaces per 1,000 sf	4 spaces per 1,000 sf	3 spaces per 1,000 sf	1 space per 1,000 sf	1 space per 5,000 sf
Correctional Institution	2 spaces per 1,000 sf	5 spaces per 1,000 sf	4 spaces per 1,000 sf	3 spaces per 1,000 sf	1 space per 2,500 sf	1 space per 5,000 sf
Hospital	2 spaces per 1,000 sf	5 spaces per 1,000 sf	4 spaces per 1,000 sf	3 spaces per 1,000 sf	1 space per 5,000 sf	1 space per 10,000 sf
Park	NA	NA	NA	NA	NA	NA
Religious Institution	2 spaces per 1,000 sf	12 spaces per 1,000 sf	8 spaces per 1,000 sf	5 spaces per 1,000 sf	NA	1 space per 2,500 sf
School, Public or Private						
Elementary and Middle	0.5 spaces per 1,000 sf	5 spaces per 1,000 sf	4 spaces per 1,000 sf	3 spaces per 1,000 sf	1 space per 2,000 sf	1 space per 1,000 sf
High	1 spaces per 1,000 sf	5 spaces per 1,000 sf	4 spaces per 1,000 sf	3 spaces per 1,000 sf	1 space per 2,000 sf	1 space per 1,000 sf
School, Vocational or Trade	2 space per 1,000 sf	6 spaces per 1,000 sf	5 spaces per 1,000 sf	4 spaces per 1,000 sf	1 space per 2,000 sf	1 space per 1,000 sf
Solar Garden	NA	NA	NA	NA	NA	NA
Transportation Facility, Public	NA	NA	NA	NA	NA	NA
University or College	1 spaces per 1,000 sf	5 spaces per 1,000 sf	4 spaces per 1,000 sf	3 spaces per 1,000 sf	1 space per 2,500 sf	1 space per 1,000 sf
Utility Facility						
Major	NA	NA	NA	NA	NA	NA
Minor	NA	NA	NA	NA	NA	NA

* N/A = Not Applicable

17.8.2: Visitor Parking

17.8.2.1: General Standards

- A. A minimum of 1 visitor parking space shall be provided with every multifamily development.
- B. All multifamily developments of 10 or more units shall provide visitor parking spaces in addition to parking for individual units.
 - 1. In the Suburban context and R-MF district, 1 visitor parking space shall be provided for every 10 units.
 - 2. In the Urban context, 1 visitor parking space shall be provided for every 15 units.
 - 3. In the Transit context, 1 visitor parking space shall be provided for every 20 units.

17.8.3: Parking Substitutions and Reductions

17.8.3.1: General Standards

- A. Motorcycle and scooter parking spaces may substitute for up to five percent of the required vehicle parking requirement.
- B. For every four motorcycle and scooter spaces provided, the vehicle parking requirement is reduced by one space.
- C. Each motorcycle and scooter space must be at least 4 feet wide and 8 feet deep. Existing parking may be converted to take advantage of this standard.
- D. At the discretion of the Director,
 - 1. On-street parking available along the portion of a public or private street abutting the use may be counted toward the minimum number of parking spaces required only if spaces are new, indented parking outside of the lanes of traffic;
 - 2. Up to a 20 percent reduction in parking may be granted for age restricted communities, where the residents are age 55 or older or for income restricted residential communities, where the average income is less than or equal to 65 percent of the area median income (AMI);
 - 3. Up to a 20 percent reduction in parking may be granted for restaurant uses located within a Transit context.
- E. Parking requirements may be met on-site or off-site at a distance of up to 600 feet from the use provided that a shared parking agreement is obtained prior to approval of the site plan or tenant improvement permit.

- F. The minimum parking count in [Table 17.8.2](#) is used to determine the parking count for each individual land use for a development application. The minimum number of parking spaces required may be reduced by the percentage indicated for each column of the five time periods in the parking reduction schedule as shown below in [Table 17.8.2](#).

The resulting parking count from [Table 17.8.2](#) for each individual land use is then determined by totaling the number of spaces in each column. The resulting column total that generates the highest total number parking spaces then becomes the new minimum parking requirement.

Table 17.8.2: Parking Reduction Schedule					
Use	Weekday		Weekend		Night-time
	6 AM - 6 PM	6 PM - 12 AM	6 AM - 6 PM	6 PM - 12 AM	12 AM - 6 AM
Residential	40%	10%	20%	10%	0%
Club, Lodge or Service Organization; Fitness or Athletic Facility; Gallery or Studio	50%	0%	0%	0%	90%
Entertainment Facility	60%	0%	20%	0%	90%
Industrial	0%	90%	90%	95%	95%
Hotel	30%	0%	30%	0%	30%
Office	0%	90%	90%	95%	95%
Public / Civic / Institutional	50%	0%	0%	70%	95%
Restaurant	50%	0%	0%	0%	90%
Retail	50%	10%	0%	30%	95%
All other uses	0%	0%	0%	0%	0%

17.8.4: Bicycle Parking

17.8.4.1: General Standards

Bicycle parking is required in order to encourage the use of bicycles by providing for safe and convenient places to park bicycles. The purpose of these design standards is to ensure that bicycle parking is convenient to bicyclists and provides sufficient security from theft and damage.

- A. Bicycle parking shall be designed so that bicycles may be securely locked and safeguarded from intentional or accidental damage.
- B. Bicycle parking areas shall be clearly identified by a sign near the main building entrance.
- C. Bicycle parking areas shall be located along the “desire line” from adjacent bikeways; the path that cyclists are most likely to travel.
- D. Each required bicycle parking space shall be accessible without moving another bicycle.

- E. There shall be an aisle at least 5 feet around the perimeter of all bicycle parking areas to allow room for bicycle maneuvering.
- F. Short-term bicycle parking shall be provided in lockers or racks that meet the standards of this Article.
- G. Short-term bicycle parking spaces shall be located within 50 feet of the main entrance to the building. With the permission of the City, bicycle parking may be located in the public right of way. Where there is more than one main entrance to a building, short-term spaces should be split between building entrances.

17.8.4.2: Long-term Bicycle Parking

Long-term bicycle parking provides users of a site a secure and weather-protected place to park bicycles. Long-term parking does not have to be provided on site, however long-term bicycle parking must be within a reasonable distance of a site in order to encourage bicycle use.

- A. Long-term bicycle parking is not required on a site when:
 - 1. Non-residential gross building area is less than 5,000 square feet; or
 - 2. There are 10 or fewer residential units in a development or redevelopment.
- B. Shower and changing facilities are required in employment-based buildings of 30,000 square feet or more where long-term bicycle parking is required.
- C. Long-term bicycle parking shall be located on the site or in an area within 250 feet of the building.
 - 1. All long-term bicycle parking shall be designed to provide maneuvering areas sufficient to prevent conflicts with other bicycles.
 - 2. Covered bicycle parking may be provided inside buildings, under roof overhangs or awnings, in bicycle lockers, or within or under other structures. When covered bicycle parking is not located within a building or locker, the cover shall be:
 - a. Designed to protect bicycles from precipitation.
 - b. High enough to provide at least 10 feet of clearance above the floor or ground.
 - c. Posted with a sign indicating the location of the bicycle parking when not directly visible at a transit facility or main building entrance.
 - 3. To provide security, long-term bicycle parking shall be in at least one of the following locations:

- a. In a locked room;
- b. In bicycle lockers;
- c. In an area that is enclosed by a fence with a locked gate;
- d. In a freestanding shelter;
- e. Within view of an attendant or security guard;
- f. Within 100 feet of an attendant or security guard;
- g. In an area that is monitored by a security camera; or
- h. In an area that is visible from employee work areas.

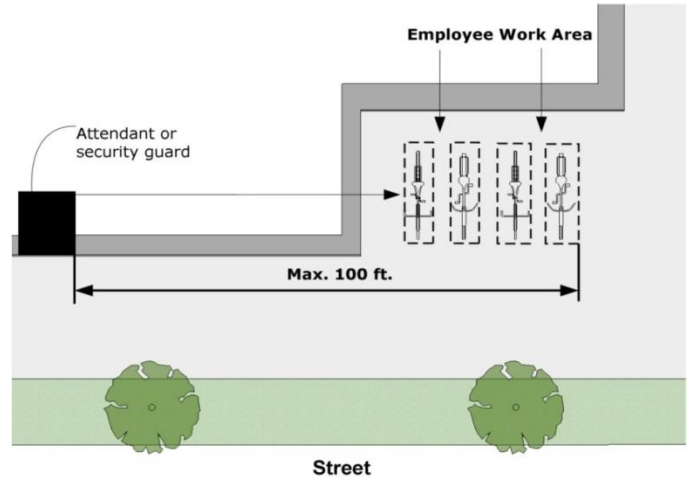


Figure 17.8.1: Long-term Bicycle Parking

17.8.5: Electric Vehicle Parking Standards

Lakewood Sustainability Plan's Goal T1 is to "Develop, maintain, and operate sustainable transportation systems and infrastructure". In order to support sustainable transportation via electric vehicles, new construction needs to include charging stations, as well as infrastructure to add more stations in the future.

17.8.5.1: General Standards

- A. All parking spaces that are required to be electric vehicle charging stations (EVCS) capable or that are required to install EVCS must meet the EVCS specifications in the Lakewood Building Code.
- B. Required EVCS capable spaces must include space, electrical conduit or cable raceway, electrical banks, and access points.
- C. Required EVCS installations should be at minimum a Level 2 electric vehicle charging station, supplying current at 240 V or 208 V.
- D. All EVCS shall include signage identifying spaces as restricted parking. For purposes of this section, "charging" means that an electric vehicle is parked at an electric vehicle charging station and connected to the electric vehicle supply equipment ports. If time limits or vehicle removal provisions are to be enforced, regulatory signage including parking restrictions shall be installed immediately adjacent to, and visible from the electric vehicle charging station.
- E. The property owner is not restricted from collecting a service fee for the use of an EVCS made available to residents, employees, and visitors to the property.

17.8.5.2: Electric Vehicle Charging Station Standards

- A. Developments that meet the following conditions must follow the standards in this section.
 - 1. New development with more than 10 off-street parking spaces; and
 - 2. The development includes one or more of the uses identified in Table 17.8.5.
- B. Electric Vehicle Charging Stations are to be included in the calculation for both the number of minimum and maximum spaces required, as provided by Table 17.8.53
 - 1. For developments that meet the threshold listed in section 17.8.5.2.A. the number of EVCS required to be installed at the time of development is stated as a percentage of the total number of new or additional parking spaces in Column A of Table 17.8.5.
 - 2. To meet anticipated demand for EVCS Column B specifies the required EVCS capable parking spaces to enable future EVCS.
 - 3. Requirements will be rounded to the nearest whole number, but will always be a value of at least one EV charging station to be available at the time of development.

Table 17.8.3: Electric Vehicle Charging Station Standards

<u>Land Use</u>	<u>A: EVCS Installed Spaces</u>	<u>B: EVCS Capable Spaces</u>	<u>C: Total Percent of EV Ready Spaces</u>
<u>Tier 1</u>			
<u>Multifamily dwelling unit</u>	<u>2%</u>	<u>18%</u>	<u>20%</u>
<u>Hotel</u>	<u>2%</u>	<u>18%</u>	<u>20%</u>
<u>Motel</u>	<u>2%</u>	<u>18%</u>	<u>20%</u>
<u>Office</u>	<u>2%</u>	<u>18%</u>	<u>20%</u>
<u>Parking, Stand-Alone</u>	<u>2%</u>	<u>18%</u>	<u>20%</u>
<u>Tier 2</u>			
<u>Group Residential Facility</u>	<u>2%</u>	<u>13%</u>	<u>15%</u>
<u>Bed and Breakfast</u>	<u>2%</u>	<u>13%</u>	<u>15%</u>
<u>Club, Lodge, or Service Organization</u>	<u>2%</u>	<u>13%</u>	<u>15%</u>
<u>Entertainment Facility (Indoor or Outdoor)</u>	<u>2%</u>	<u>13%</u>	<u>15%</u>
<u>Golf Course</u>	<u>2%</u>	<u>13%</u>	<u>15%</u>
<u>Community Building</u>	<u>2%</u>	<u>13%</u>	<u>15%</u>
<u>Convention or Exposition Center</u>	<u>2%</u>	<u>13%</u>	<u>15%</u>
<u>Correctional Institution</u>	<u>2%</u>	<u>13%</u>	<u>15%</u>
<u>Hospital</u>	<u>2%</u>	<u>13%</u>	<u>15%</u>
<u>Religious Institution</u>	<u>2%</u>	<u>13%</u>	<u>15%</u>
<u>School, Public or Private, Elementary and Middle and High</u>	<u>2%</u>	<u>13%</u>	<u>15%</u>
<u>School, Vocational or Trade</u>	<u>2%</u>	<u>13%</u>	<u>15%</u>
<u>University or College</u>	<u>2%</u>	<u>13%</u>	<u>15%</u>

17.8.6: Single-family, Duplex and Attached Residential Parking Standards

17.8.6.1: Driveways and Parking Areas

The following standards shall apply to all single-family and duplex residential lots:

- A. On single-family and duplex residential (R) zoned lots, driveways and parking areas shall not exceed 50 percent of the back yard, 50 percent of the front yard, and 50 percent of the side yard.
- B. Each lot shall be allowed no more than 35 feet of drive-cuts along a lot's street frontage and may occupy no more than 50 percent of the lot frontage, cul-de-sacs excluded.

- C. Driveways and parking areas are to be maintained as dust-free, weed-free, and mud-free surfaces.
- D. Driveways and parking areas shall not be allowed in location intended for other purposes such as landscaping or open space.
- E. Parking shall not be allowed on grass, weeds, mud or dirt. This includes, but is not limited to, the parking of trailers, campers and camper shells, and recreational vehicles.
- F. Approved all weather parking surfaces for detached single-family include concrete paving, asphalt paving and rock applied to a minimum depth of 3 inches. Rock driveways and parking areas shall use a minimum $\frac{3}{4}$ inch rock size.
- G. All weather surfaces shall not include materials including but not limited to carpet, shingles, wood or cardboard.
- H. A property owner shall comply with the requirement for an improved parking surface within 30 days of a posting or receipt of a notice of violation of subsection 17.6.3.2.

17.8.7: Multifamily, Institutional, Mixed-Use, Commercial, and Light Industrial Parking and Loading Standards

17.8.7.1: Americans with Disabilities Act Parking Standards

All places of public accommodation must comply with the Department of Justice published revised regulations for Titles II and III of the Americans with Disabilities Act (ADA) 42 U.S.C.S. 12101, et. seq.

17.8.7.2: Loading Space Standards

This Section establishes off-street loading space standards which provide requirements for the design and construction of loading areas.

- A. At no time may loading or unloading occur from the right-of-way of a collector or arterial street.
- B. Whether or not a loading space is provided, all vehicle maneuvering for loading or unloading shall occur on site.
- C. A loading space shall not encroach on or interfere with the public use of streets and sidewalks by vehicles and pedestrians.
- D. No loading space shall permit any vehicle to extend into any front setback area or across any lot line of a more restrictive district while being loaded or unloaded.
- E. Loading spaces shall not conflict with or overlap any required drive aisles or off-street parking spaces, unless the loading space will only be used during hours when the primary structure is not open for business.

17.8.7.3: Sight Triangles

For information pertaining to the measurement of sight distance triangles, please refer to the Transportation Engineering Design Standards (TEDS) as amended.

17.8.7.4: Vehicle Stacking

Vehicle stacking is the minimum length required for an on-site drive aisle necessary to facilitate the safe movement of vehicles between the parking lot and the public street; and/or the minimum required length of an on-site drive aisle necessary to facilitate movement of vehicles within a parking lot to drive-up window service or other drive-through services.

- A. Adequate space must be provided for on-site stacking, storage and queuing of vehicles.
- B. The required stacking distance for the site may be distributed between access points serving the site, provided a minimum stacking of 20 feet is provided at all access points.
- C. Stacking spaces must be a minimum of 8 feet in width and 20 feet in length.
- D. Vehicles using drive-thru facilities shall not encroach on or interfere with the on-site or off-site use of streets, and sidewalks by vehicles and pedestrians.
- E. Stacking spaces for internal drive-through services shall be measured from the point of service and within a designated drive aisle (See Figure 17.8.2). Stacking spaces are shown in [Table 17.8.34](#).

Table 17.8.4: Drive-Through Stacking	
Type of Facility	Minimum Number of Stacking Spaces
Car Wash, Automatic	2
Car Wash, Self-Service	2
Dry Cleaner, Drive-Through	2
Financial Institution, Drive-Through	1
Gasoline Pump Island	1
Liquor Store, Drive-Through	2
Restaurant, Drive-Through	4
Other	Determined by the Director with queuing study

- F. Stacking distances for individual parking lots are indicated in [Table 17.8.4](#) (See Figure 17.8.3).

Table 17.8.5: Vehicle Stacking at Entrances	
Number of Parking Lot Spaces	Stacking Distance in Feet
0 to 100 spaces	20
101 to 500 spaces	40
501 to 1000 spaces	60
1001+ spaces	100

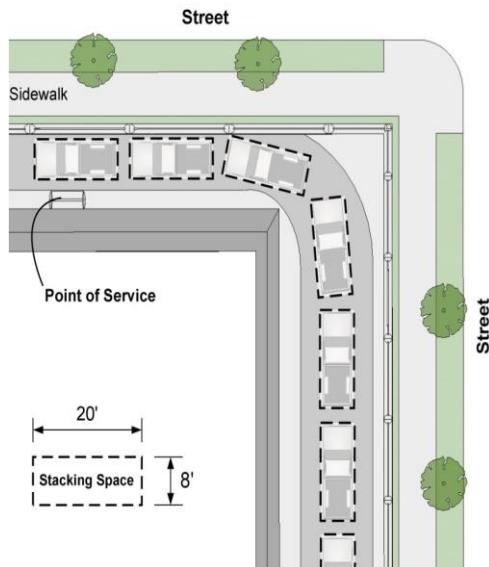


Figure 17.8.2: Drive -Through Stacking

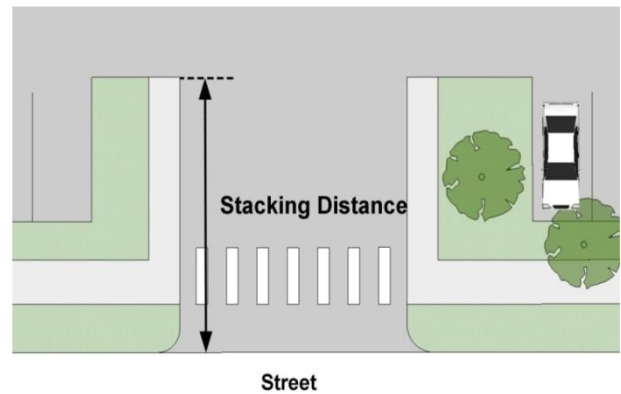


Figure 17.8.3: Stacking Distance

17.8.8: Surface Parking Lot Dimensions

17.8.8.1: General Standards

The following standards are intended to create landscaped surface parking areas that are easy to navigate for pedestrians and vehicles.

- A. Vehicular drive aisles, parking areas, stacking areas, and loading areas shall be surfaced with asphalt, concrete, brick, stone pavers or an equivalent material including pervious materials. Gravel or other similar compacted materials are not an acceptable parking surface.
- B. Parking space dimensions for parking spaces in parking structures may differ from [Table 17.8.2](#) as part of a site plan review as outlined in Article 2 of this Zoning Ordinance.
- C. Parking spaces shall be defined on the pavement surface with striping, change of color or material.
- D. Bumper blocks are not permitted in parking lots except to provide for separation between an ADA accessible parking space and a sidewalk or where needed to provide for surface flows to a storm water management facility or at the discretion of the Director.
- E. Parking lots and loading areas shall have access from a clearly defined drive aisle not less than 18 feet in width for one-way traffic and 24 feet in width for two-way traffic.
- F. The size of a parking stall, its angle, and the width of the access aisle shall conform to the parking layout dimensions listed in [Table 17.8.56](#) and illustrated in Figure 17.8.4.

Table 17.8.6: Parking Lot Layout Dimension						
Dimension	Figure 17.8.4	0°	45°	60°	75°	90°
Stall width, parallel to aisle	A	9.0	12.7	10.4	9.3	9.0
Stall length of line	B	24.0	24.5	21.5	19.5	18.0
Stall depth to wall	C	9.0	17.0	18.5	19.0	18.0
Aisle width between stall lines	D	12.0	12.0	16.0	22.0	24.0
Stall depth interlock	E	9.0	14.8	17.0	18.3	18.0
Module, wall to interlock	F	30.0	43.8	51.5	59.3	60.0
Bumper overhang (typical)	G	0.0	1.5	1.8	2.0	2.0
Offset	H	--	6.3	2.7	0.5	0.0
Setback	I	24.0	11.0	8.3	5.0	0.0
Cross aisle one-way	J	18.0	18.0	18.0	18.0	18.0
Cross aisle two-way	K	24.0	24.0	24.0	24.0	24.0

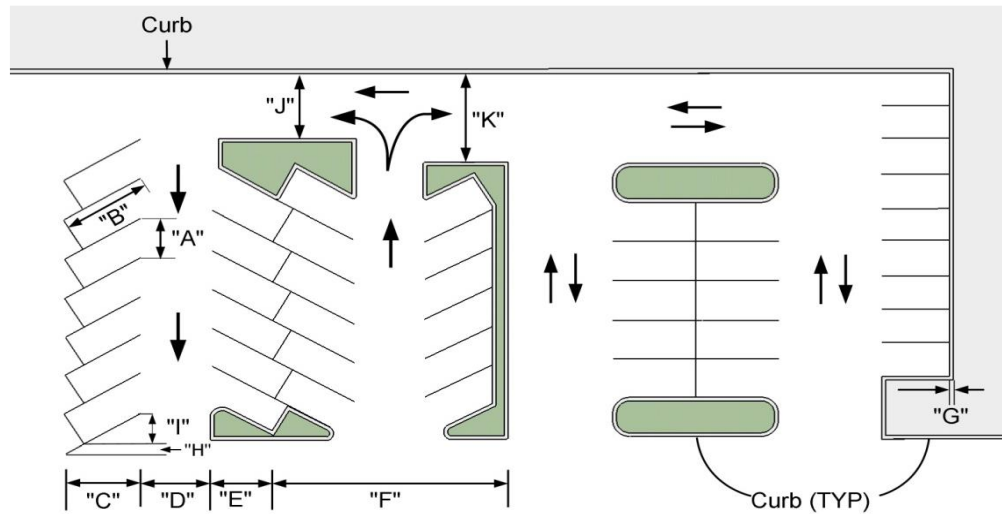


Figure 17.8.4: Parking Lot and Layout Dimensions

- G. Dead end aisles shall only be permitted for 90-degree parking layouts. An area of 5 feet in depth and the same width as the aisle shall be provided as the end of the dead end aisle to allow vehicles to safely maneuver.
- H. The minimum length of a parking stall which is perpendicular to a landscaped area may be reduced by 2 feet provided suitable ground cover is placed behind the curb a minimum distance of 2 feet.
- I. All new parking lot structural sections must be designed by a professional engineer specializing in the geo-technical field, registered in the State of Colorado, based on a soils report and shall reflect traffic volume and vehicle types.
 - 1. The minimum cross section under any condition shall be 2.5 inches of hot bituminous pavement and 4 inches of aggregate base course (Class VI) on 6 inches compacted sub-grade or 4 inches of non-reinforced Portland Cement concrete pavement on 6 inches compacted sub-grade. An equivalent full depth section over compacted sub-grade may also be used.
 - 2. A special inspector as defined and provided for in the Lakewood Building Code shall certify after field inspection, that the construction of the parking lot conforms with the approved plans prior to the issuance of a Certificate of Occupancy or final inspection.
 - 3. When a parking lot is part of an approved drainage plan, an engineer registered in the State of Colorado shall certify the construction, paving or repaving complies with the approved drainage plan.
 - 4. The maximum grade within parking lots shall be six percent; the maximum cumulative grade break must not exceed eight percent.

17.8.9: Parking Lot Placement and Design

17.8.9.1: Parking Lot Location

Surface parking lots in mixed-use zone districts shall be located in the configurations identified in Table 17.8.6 and Figure 17.8.5.

Table 17.8.7 Surface Parking Lot Location			
Regulations	Contexts		
	Suburban	Urban	Transit
X = Applicable -- = Not Applicable			
Parking may be located behind the rear plane of a building.	X	X	X ⁽¹⁾
Parking may be located at the side of a building.	X	X	--
Parking may be located in the area between the street and building.	X	--	--
⁽¹⁾ Within the M-C-T zone district, surface parking areas shall be limited to short-term, convenience parking lots with fewer than 10 spaces per building. All other parking shall be accommodated for in parking structures. Convenience spaces may be located behind or to the side of a building and shall be clearly marked for short-term use only.			

17.8.9.2: Parking Blocks

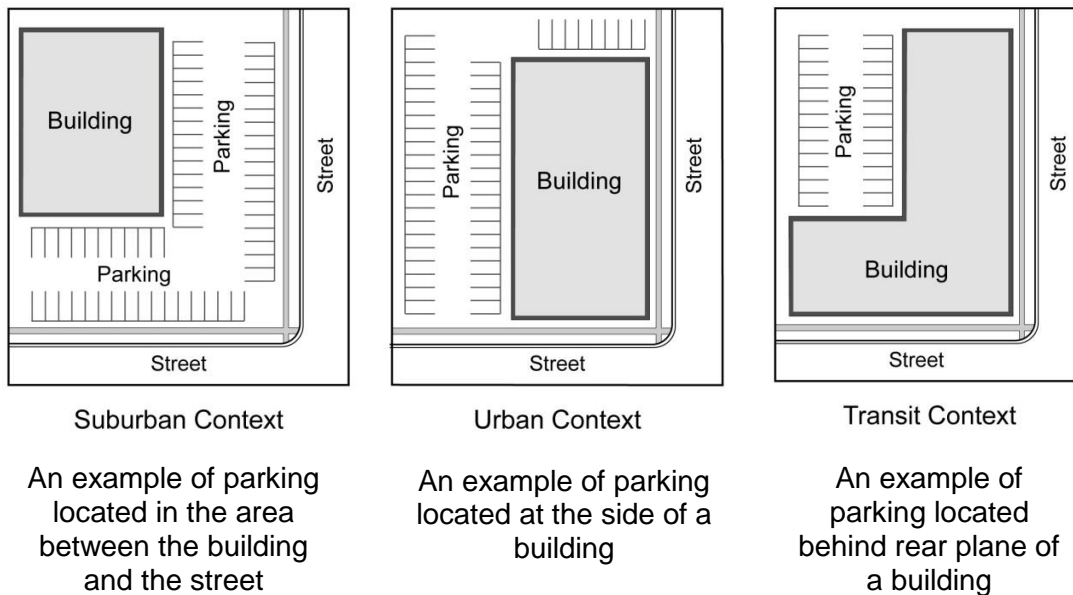


Figure 17.8.5: Example Parking Locations by Context

- A. Parking lots in blocks of 25 or fewer parking spaces shall be required to provide a sidewalk at least 5 feet in width from the parking lot to the front of the primary building(s), to the farthest perimeter point of the parking lot, or to an existing parking lot connection.
- B. Parking lots in blocks of more than 25 parking spaces shall be grouped into blocks of parking spaces according to the following:
 - 1. Multifamily developments shall group the parking lots in blocks of no more than 50 contiguous parking spaces. These spaces may be in a linear row or two or more parallel rows. A landscaped area of at least 12 feet wide shall separate parking areas (See Figure 17.8.6).
 - 2. Institutional, mixed-use, commercial or light industrial developments shall group the parking lots in blocks that average no more than 75 parking spaces per block.
 - a. A grade-separated sidewalk at least 5 feet in width shall be installed from the front of the primary building(s) to the farthest perimeter point of the parking lot. Additional grade-separated sidewalks at least 5 feet in width from the front of the primary building(s) to the farthest edge of the parking lot shall be required to ensure that no parking space is located more than 200 feet from a grade-separated sidewalk leading to the front of the primary building(s).
 - b. The grade-separated sidewalk shall be buffered from parking or traffic by a landscape strip with a minimum of 5 feet in width. The sidewalk shall be placed so that a 5-foot-wide planting area is created (See Figure 17.8.7).
 - 3. Sidewalk crossings shall be clearly defined and marked through a change in paving materials, height, or use of distinctive color when a sidewalk crosses a parking lot or internal street or driveway.

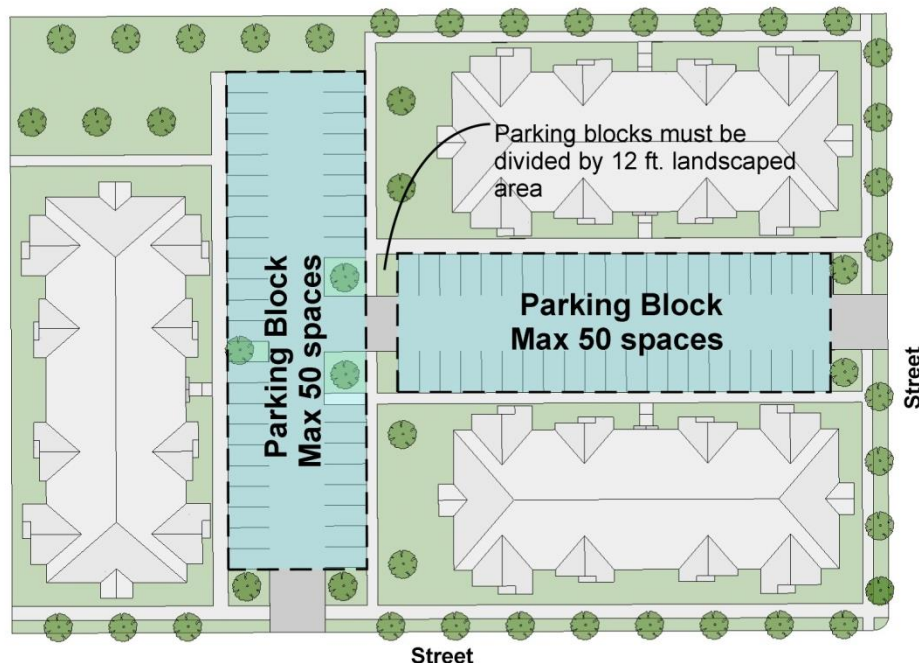


Figure 17.8.6: Multifamily Residential Parking Lot Design

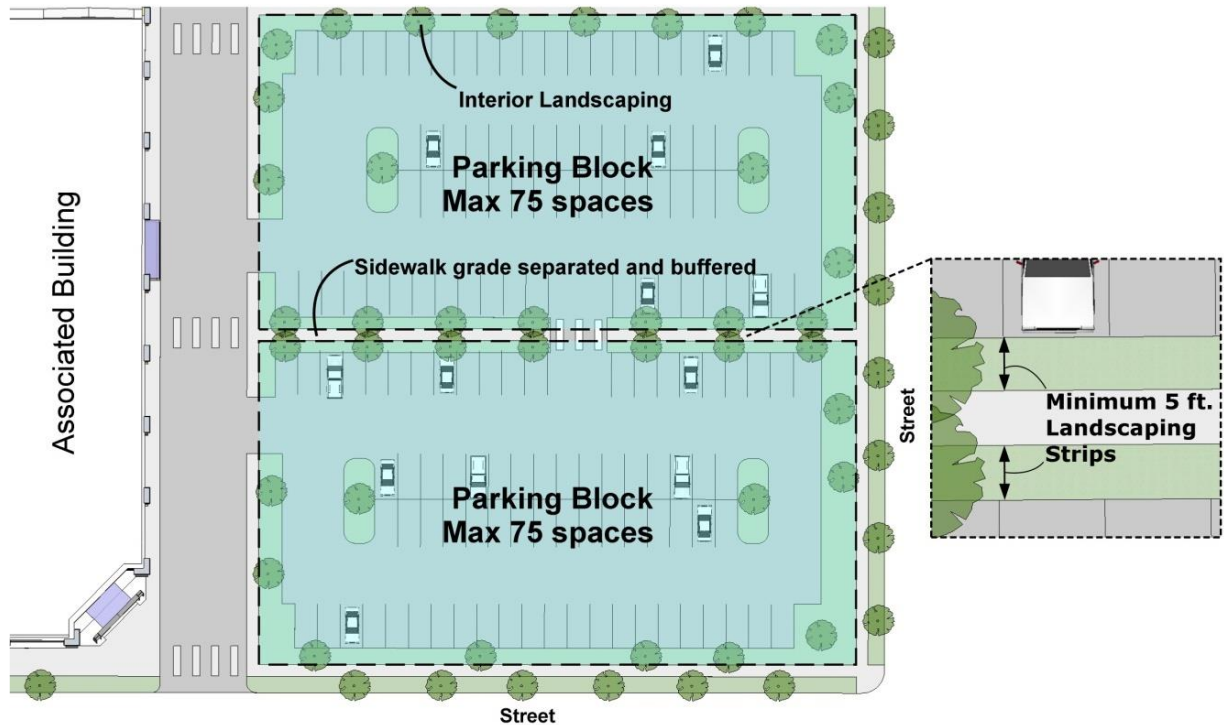


Figure 17.8.7: Mixed-use, Commercial and Light Industrial Parking Lot Design

17.8.10: Parking Lot Landscape Standards

17.8.10.1: Landscaping in Parking Lots

- A. Raised curb islands shall be required to define the ends of each parking row.
- B. The perimeter of the parking lot and any raised curb islands shall have concrete curb. Gaps in a concrete curb are allowed if landscape and open space areas or islands have been designed to provide for infiltration and filtration of rainwater.
- C. Sites requiring more than 25 parking spaces shall be required to have the following amount of landscaping in parking lots:
 1. For parking lots with fewer than 150 parking spaces, landscaping islands shall be a minimum of 10 percent of the parking area.
 2. For parking lots with 150 parking spaces or more, landscaping islands shall be a minimum of 12 percent of the parking area.
- D. The size and number of landscape islands shall be required as identified below. These requirements shall not apply when a row of parking spaces is located under a structure or at the end of a parking row that coincides with a required front, side or rear buffer:

1. A parking row containing fewer than 15 contiguous parking spaces shall be terminated by a landscape island with a minimum dimension of 9 feet in width by 18 feet in length.
2. A parking row containing between 15 and 30 contiguous parking spaces shall be:
 - a. Terminated by a landscape island with a minimum dimension of 12 feet in width by 18 feet in length (See Option A in Figure 17.8.8); or
 - b. Terminated by a landscape island with a minimum dimension of 9 feet in width by 18 feet in length and shall contain one landscape island in the middle of the row with a minimum dimension of 9 feet in width by 18 feet in length (See Option B in Figure 17.8.8).

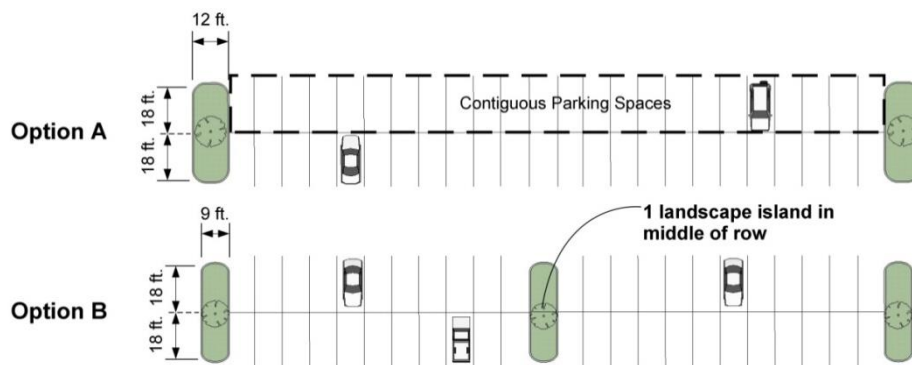


Figure 17.8.8: Parking Lot Landscape Island Locations

3. A parking row containing over 30 contiguous parking spaces shall include islands with a minimum dimension of 9 feet in width by 18 feet in length every 15 parking spaces.
4. Landscape islands shall include at least one shade tree or two ornamental trees and a landscape surface of turf, living ground cover, or a minimum of 4 shrubs or 6 grasses in mulch beds.

17.8.10.2: Screening of Parking Lots

A. When adjacent to a public or private street, surface parking shall be screened through one or any of a combination of the following (See Figure 17.8.9)

1. Option A: A landscape hedge or other plant materials of such size, branching density, spacing and quantity to provide a minimum of 60 percent opacity while dormant. The landscape hedge or plant material shall reach a required minimum height of 42 inches within three years of planting; or
2. Option B: A solid wall providing screening to a height of 42 inches. Materials utilized shall match the first floor exterior material used on the primary building or comparable level of quality; or
3. Option C: A combination of a decorative fence to a height of 42 inch, and continuous landscape and plant materials. The decorative fence shall terminate with a structural column that utilizes materials that match the first floor exterior material used on the primary building or materials of a comparable level of quality. A structural column utilizing materials that match the first floor exterior material used on the primary building or of a comparable level of quality, shall be constructed every 30 feet of linear fence; or
4. Option D: A landscaped berm containing at least one row of shade trees spaced evenly every 15 feet or as appropriate to the selected species along the entire length of the parking lot edge.

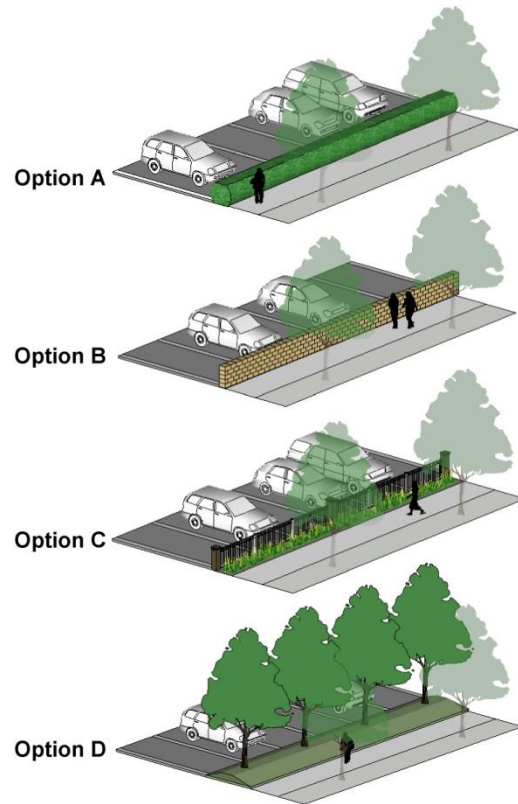


Figure 17.8.9: Screening of Parking Lots

B. Breaks in the wall, hedge, or fence shall be permitted where pedestrian access to the adjacent sidewalk is provided from the site. The breaks shall be no more than 2 feet wider than the sidewalk width