



## Lakewood **Innovates**

**Lakewood will invest in its future by proactively pursuing a thriving and sustainable economy with a variety of employment, investment, and mixed-use opportunities.**

**Citywide Economic Development Strategies**

**Growth Areas**

**Community Activity Areas**

**Neighborhood Activity Areas**

**Future Activity Areas**

**Industrial Areas**





## Guiding Principle

**Lakewood will invest in its future by proactively pursuing a thriving and sustainable economy with a variety of employment, investment, and mixed-use opportunities.**

Lakewood will have a strong, competitive, and diverse economic base with a mix of locally owned, national and international employers. Lakewood will encourage a unique blend of products and services for its residents, visitors, and businesses. The City will promote Downtown Lakewood, and existing and planned growth areas as the primary locations for new employment and residential growth. The City will encourage efficient links between neighborhoods, commercial areas, and jobs. Lakewood will promote strategic investments, partnerships with the business community, and wise stewardship of existing employment assets.

## Introduction

Lakewood, and the entire Denver metropolitan area, have seen strong economic growth in the years following the 2007-2009 national recession. Significant additional growth is anticipated in the future as well. According to the Denver Regional Council of Governments (DRCOG), employment in the Denver region will reach 2.3 million by 2035. Employment in Lakewood should reach 107,000 by the same year. Along with an increasing number of jobs comes an increase in population. The Denver region is expected to add 1.2 million people by 2035, with Lakewood growing by approximately 47,000 people.

Lakewood's economic base is its lifeblood. A growing economic and residential base provides the framework on which the city prospers and develops. Economic and residential growth is critical to the health of the city, as both generate taxes to pay for municipal services. Employment growth also provides necessary jobs for the residents of Lakewood. *Lakewood Innovates* addresses the importance of maintaining and enhancing an environment in which existing businesses can expand and new businesses are attracted. This chapter also provides direction as to where new employment and residential growth should be focused. Allowing increased residential and employment densities within designated growth areas will protect existing stable neighborhoods while allowing the city to continue prospering.

**Table 5-a: Projected Population & Employment Growth**

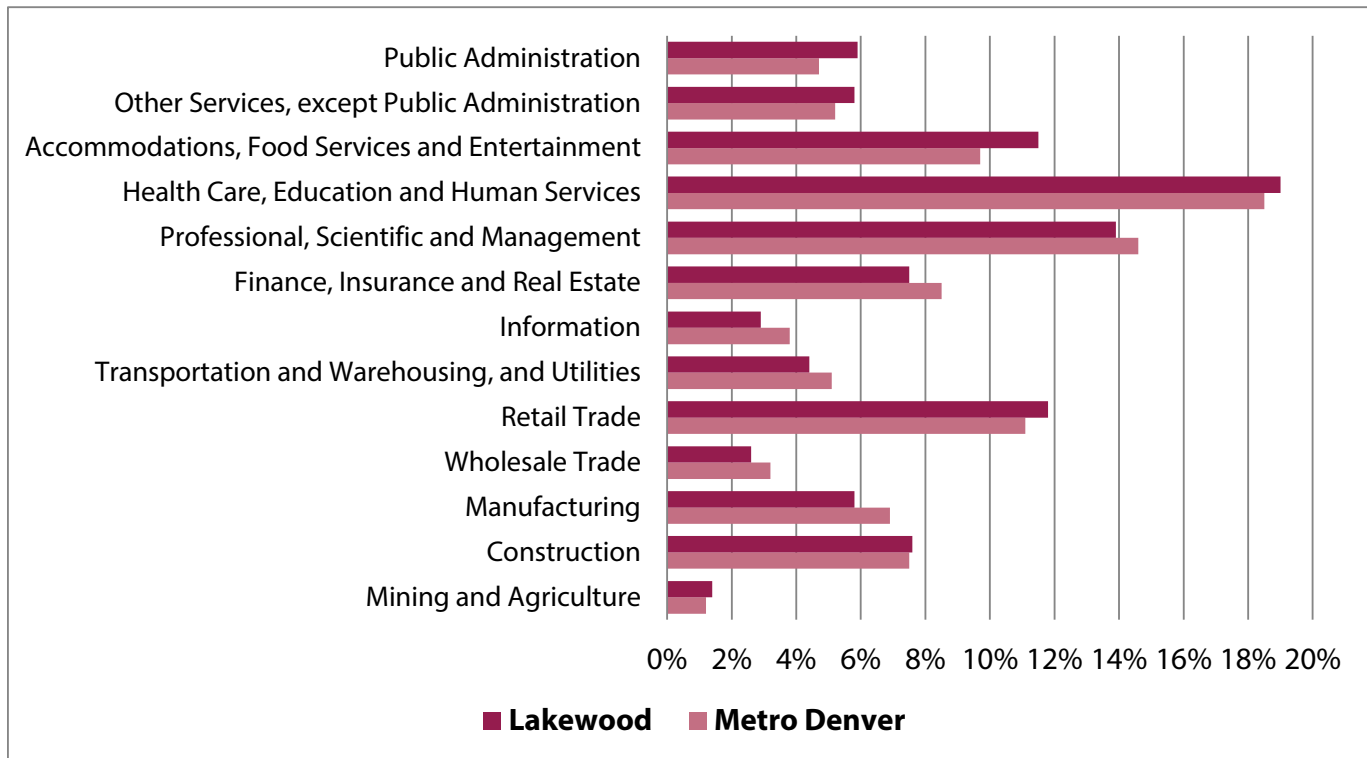
<b>Lakewood</b>	<b>2010 Estimate</b>	<b>2035 Forecast</b>
Population	143,000	190,000
Employment	73,500	107,000
<b>Denver Metro</b>	<b>2010 Estimate</b>	<b>2035 Forecast</b>
Population	2.8 Million	4.0 Million
Employment	1.6 Million	2.3 Million

Source: Denver Regional Council of Governments, 2013

## Employment

Lakewood has a diverse economy, with a significant federal government presence due to the Denver Federal Center. Overall employment within the city has increased moderately since 2000, with approximately 72,000 jobs within the community in 2012. Education and health care employ the largest percentage of people in Lakewood, followed by professional services and retail trade. The table below shows the employment composition of Lakewood in comparison to the overall Denver region.

**Figure 5-a: Comparison of Denver Metro & Lakewood Employment Composition**



Source: U.S. Census Bureau 2012 American Community Survey





In addition to the diversity of employment composition, Lakewood also boasts a diversity of employers. The city's largest employers include those in the government, education, health care and manufacturing industries. Overall, the city has more than 4,800 active businesses. The largest employers are listed below.

**Table 5-b: Lakewood's Largest Employers**

Rank	Company	Product/Service	Employees in Lakewood
1	United States Government	Government	8,000
2	St. Anthony Hospital	Health Care	2,800
3	Terumo BCT	Medical Device Manufacturing	2,309
4	Jefferson County Public Schools	Education	2,300
5	FirstBank	Financial Services	950
6	City of Lakewood	Government	893
7	Home Advisor	Technology	789
8	The Integer Group	Marketing	632
9	State of Colorado	Government	547
10	Kaiser Permanente	Medical	500

Source: *City of Lakewood, 2014*

The inventory of employment space - office, retail and industrial - within the city has continued to grow slowly, but consistently since 2000.

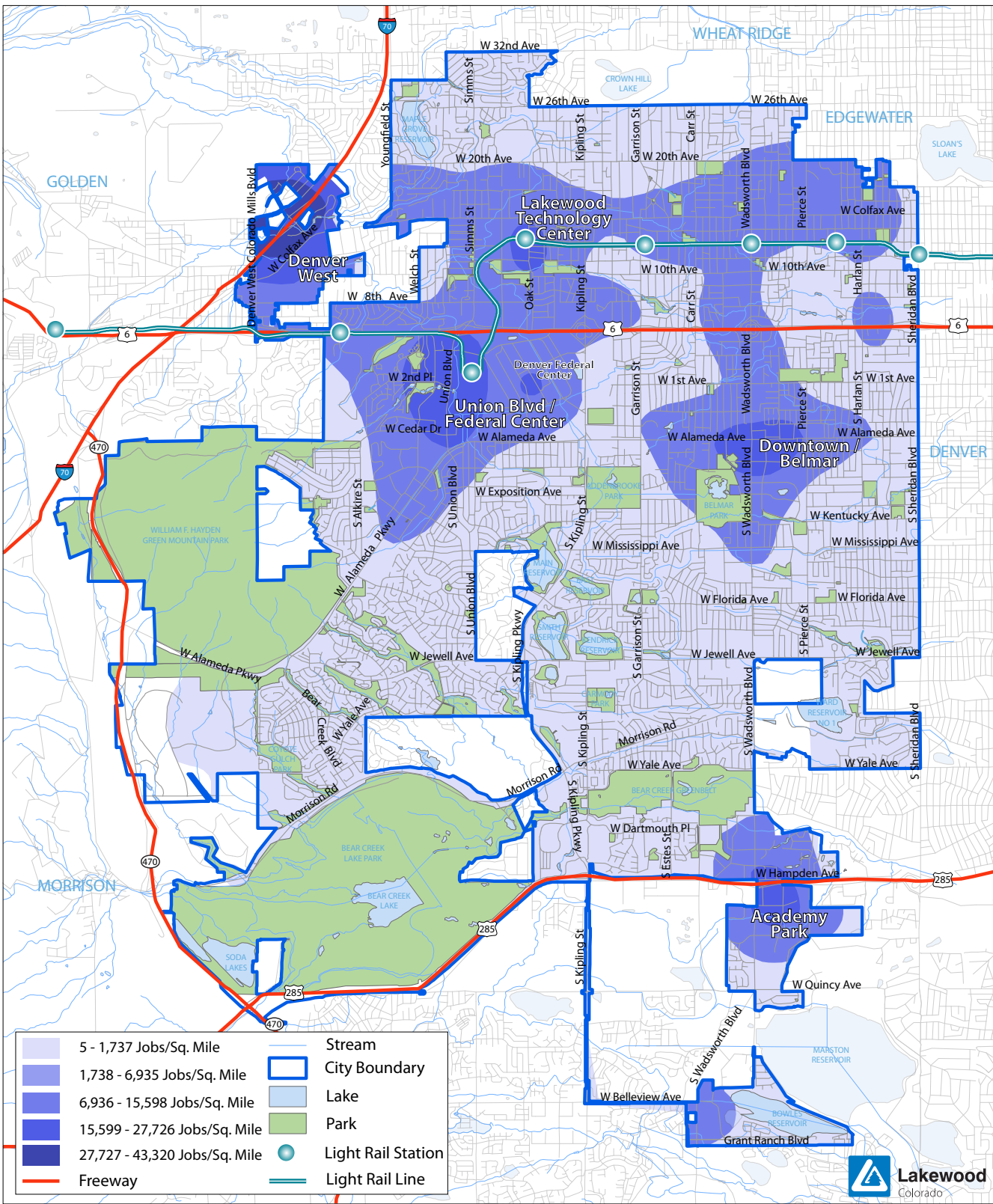
**Table 5-c: Inventory of Employment Space Between 2000 & 2014**

	2000	2014	Change
<b>Industrial</b>	3,194,539 Square Feet	3,431,163 Square Feet	7%
<b>Office</b>	9,836,222 Square Feet	10,824,268 Square Feet	10%
<b>Retail</b>	8,032,463 Square Feet	9,222,955 Square Feet	15%

Source: *City of Lakewood, 2014*

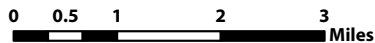
The increases in space generally have been related to major economic development projects within the city, such as the redevelopment of the Villa Italia Mall into Belmar; construction of St. Anthony Hospital and associated medical office buildings; continued development in the Denver West area including the Colorado Mills Mall; and construction of Lakewood City Commons and the City's Civic Center facilities.

Several other new projects will add to these inventories over the short-term, including expansion of the First Bank headquarters, the construction of the Terumo BCT headquarters building, and additional office and hotel development in Belmar.



## Map 5-a

## Employment Density



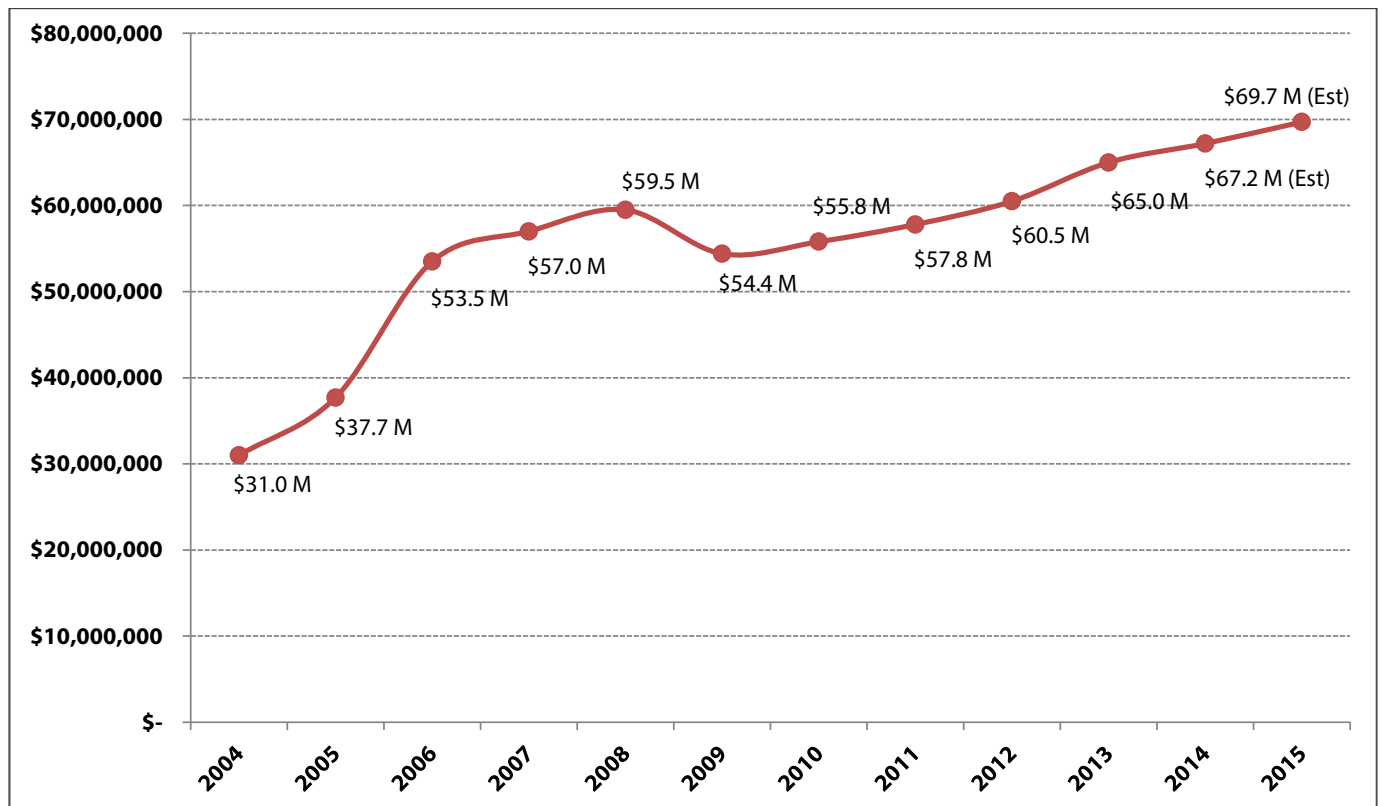


## Sales Tax

Sales tax revenue is an important component of the City's budget, as this revenue is the single largest contributor to the general fund. Therefore, it is vital to continue to strengthen the city's retail market and encourage new sales tax producing businesses to locate in Lakewood.

Sales tax revenue has generally risen since 2004, with a pronounced increase following a voter-approved sales tax increase in 2005 and a three year dip following the 2008 recession.

Figure 5-b: Sales Tax Revenue



Source: City of Lakewood

## Economic Development & Reinvestment

The City's economic development strategy will be to continue to focus on maintaining and cultivating an innovative, diverse, and sustainable economy. A range of office, retail, and light manufacturing uses will continue to be pursued to help the City support its residents and to help weather future economic fluctuations. The City will also focus on growing existing businesses and industries to help expand the primary employment base. A focus on supporting and nurturing the city's smaller businesses will also continue.

Additionally, in today's talent-driven economy, the city's ability to capture new opportunities will depend somewhat less on having the lowest business costs or lowest taxes than on having a higher "quality of life" than its competitors. Although "quality of life" is difficult to gauge, how Lakewood measures up in categories such as personal safety and security, neighborhood integrity and choice, recreational opportunities, availability of retail and entertainment opportunities, and quality schools and higher learning is important in helping to attract and retain people and businesses.

As a tool to attract new businesses and residents to revitalizing areas, the City has established three urban renewal districts administered by the Lakewood Reinvestment Authority.

The *West Alameda Avenue Corridor Reinvestment Area* has been a major catalyst in transforming land and creating a true downtown for Lakewood. Since the area's designation, the LRA has helped bring about the redevelopment of the Villa Italia Mall into Belmar, the city's premier mixed-use neighborhood and the focal point of the downtown area.

The *Colfax-Wadsworth Reinvestment Area* is an area once occupied by a failed big box retailer. This high-traffic retail site sat vacant for more than a decade due to challenges associated with fractured ownership and drainage issues. These challenges were overcome in 1999 when the site was designated as an urban renewal area. The development in this area is the Creekside Shopping Center with a Walmart store anchor.

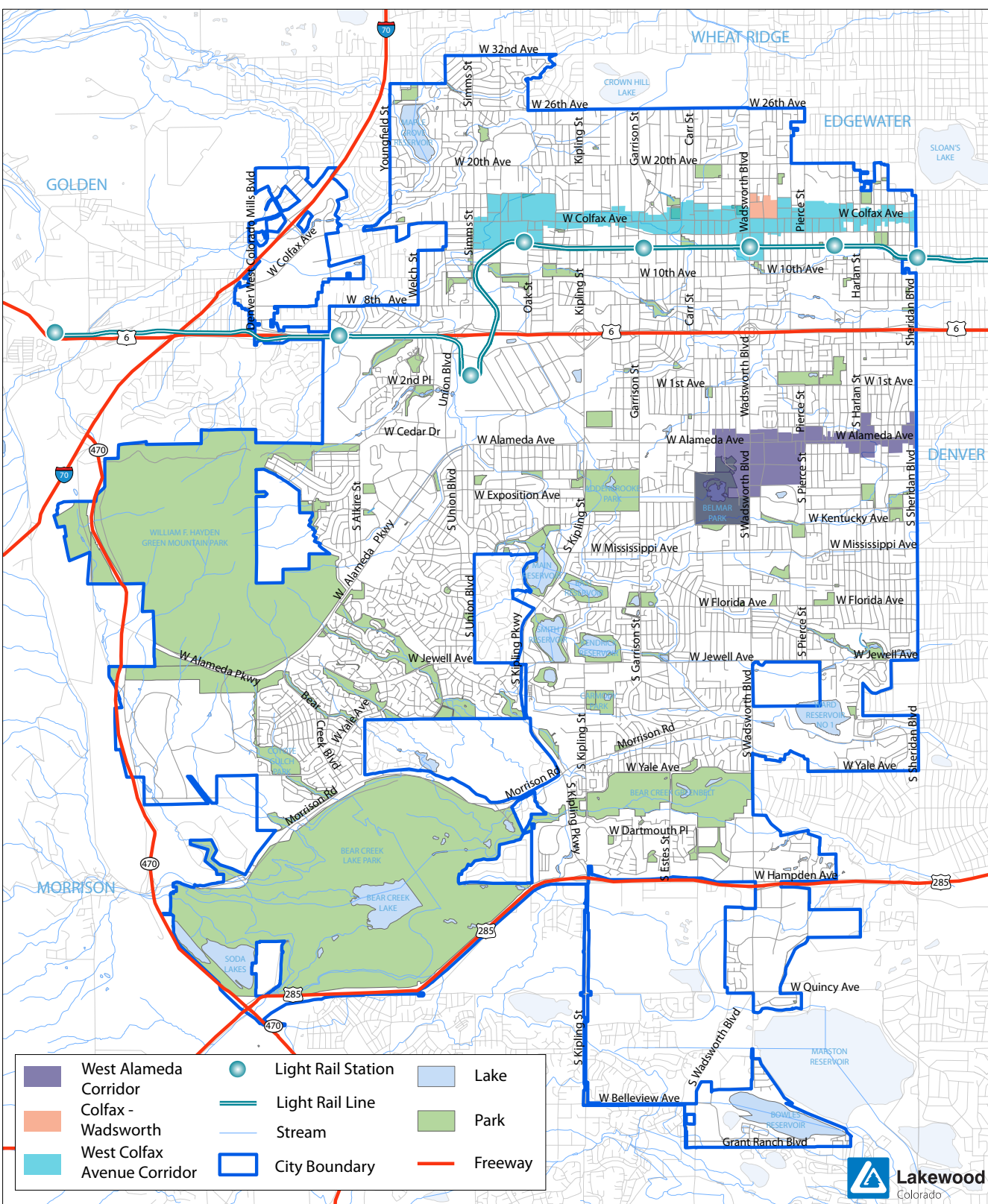
The *West Colfax Avenue Corridor Reinvestment Area* spans from Sheridan Boulevard to Simms Street, mainly following the commercial properties that front Colfax Avenue. The area also includes properties surrounding or near several West Rail Line stations. Station platform and design betterments at the Lakewood-Wadsworth and Oak stations have resulted from this area designation.

These areas will continue to provide a reinvestment and economic development tool for these locations in the City where increased business, employment, and residential densities are planned and expected to occur.

In the future, it may be appropriate for the City to consider other areas for urban renewal, using the goals and action steps identified in this Plan as a guide.







Map 5-b

## Lakewood Reinvestment Areas (LRAs)

0 0.5 1 2 3 Miles



## Business Organizations

Cooperation with private sector businesses and business organizations is also a key piece of economic development within Lakewood. These organizations have spearheaded a number of economic development efforts in conjunction with the City, including the creation of two business improvement districts (BIDs) and the 40 West Arts District. These organizations will continue to help increase the economic potential of the city and work towards business attraction and retention, as well as investing in public infrastructure and art.

There are currently five business organizations registered with the city.

*The Alameda Gateway Community Association (AGCA)*, which promotes the Alameda Avenue corridor between Denver and C 470. The organization has been actively involved with development in Downtown Lakewood and supportive of Federal Center redevelopment efforts. The organization also successfully formed the *Alameda Gateway Business Improvement District* located between Sheridan Boulevard and Carr Street.

*The Applewood Business Association* represents businesses in the northwestern portion of Lakewood, as well as portions of Wheat Ridge, Golden and unincorporated Jefferson County. The Applewood Business Association is dedicated to promotion, expansion, and development of the area by providing services that aid, support, and promote business and by expressing and supporting the protection, identity, and growth of Applewood businesses and the community.

*The South Lakewood Business Association* promotes business development in the southern area of Lakewood, with a focus on the area surrounding the Jewell Avenue and Wadsworth Boulevard intersection. The organization is dedicated to bringing together the South Lakewood business community by providing a means for communication, establishing a network, and becoming more involved with the city and people of Lakewood.

*The Union Corridor Professionals Group* represents businesses in the western portion of Lakewood, with a focus on the Union Boulevard corridor between US 6 and Alameda Avenue. The organization is affiliated with The West Chamber.

*The West Colfax Community Association (WCCA)* promotes investment and development along the Colfax Avenue corridor between Denver and Simms Street and along the West Rail Line. The organization has facilitated the creation of the 40 West Arts District and the *Lakewood West Colfax Business Improvement District*.

### What is a Business Improvement District?

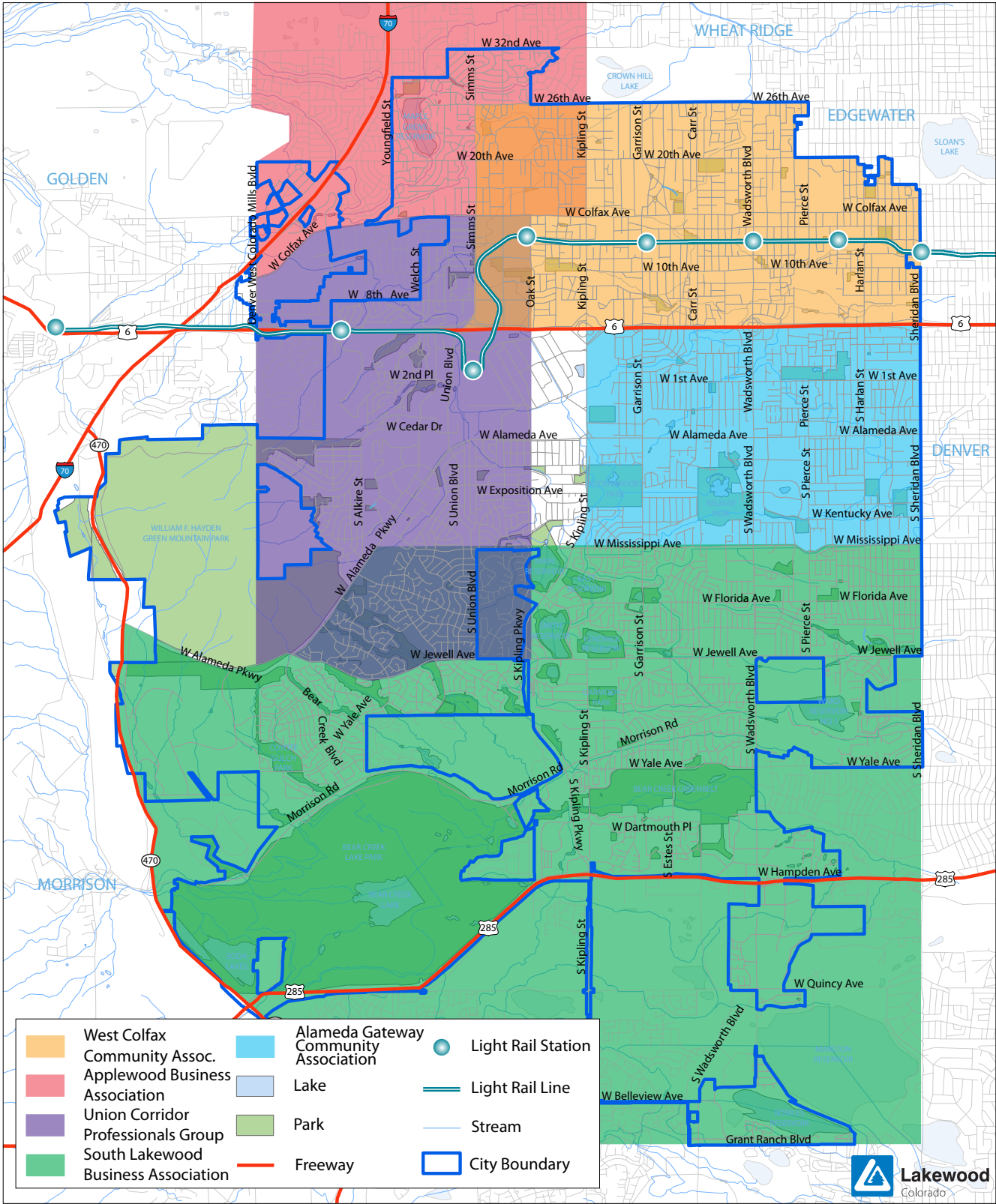
A Business Improvement District (BID) is a public/private partnership in which property and business owners elect to make a collective contribution to the maintenance, development and promotion of their commercial district.

The idea for BIDs is modeled on the shared maintenance program of many suburban shopping centers where malls are typically single properties, managed by one entity. Tenants pay a common area maintenance fee to underwrite services that enhance the appearance of common areas and provide cooperative advertising for the mall and its stores.

A BID works in much the same way. However, because the BID is set in an urban context, multiple property owners agree to the extra fee or tax (assessment). Thus, stakeholders in a commercial district can align themselves in much the same way to improve and maintain their area.

BIDs have specifically defined boundaries where fees or taxes are assessed and where maintenance, improvement, and marketing activities are focused.





The projected increases in employment and population in Lakewood will have a positive impact on sales tax revenue as long as both employment and residential growth can be accommodated. Projected growth will also allow the city's economic base to support the needs of the community, offer jobs in growing industries that currently are located in the city, and increase the retail opportunities available within the key growth and activity areas described later in this chapter.





# CITYWIDE ECONOMIC DEVELOPMENT STRATEGIES

## Overview

The purpose of the citywide economic development strategies is to enhance the city's employment base, support existing retail, and encourage new retail businesses to locate in the community. The citywide strategies are broad in nature and should be applied holistically across the community. The citywide strategies also focus on opportunities to attract regional and out-of-state tourism business, and on opportunities to enhance relationships with existing area employers and higher education facilities.

In addition to encouraging new retail development in appropriate locations, fostering reinvestment in aging commercial areas of the city is also vital to the overall economic health of the community and neighborhoods adjacent to those centers. The city also provides a healthy and dynamic environment for business, and has attracted a number of medical, technology and retail employers in recent years.

Although Lakewood has an attractive location in the Denver region, the City must maintain partnerships with existing businesses and employers and pursue economic development opportunities that capitalize on the community's strengths.

This section identifies goals and action steps that will encourage new development and redevelopment in a manner that moves the city forward, giving Lakewood a competitive edge in attracting and keeping residents and businesses.



### Industries Targeted by Lakewood for Economic Development Efforts

The following industries are being targeted by Lakewood for economic development recruitment:

- Aerospace
- Aviation
- Bioscience
- Broadcasting and Telecommunications
- Energy
- Financial Services
- Healthcare and Wellness
- Information Technology
- Software Development
- Creative Industries

**GOAL I-CS1**

**Encourage investment in Lakewood that creates new employment opportunities and advances the City's economic goals.**

**ACTION STEPS**

- a. Promote Lakewood as a premier destination for metro Denver's target economic development industries.
- b. Develop criteria by which to prioritize infrastructure improvements that support economic investment in addition to other community-wide goals.
- c. Continue to provide staff support to business organizations within the City to promote and assist with economic development.
- d. Support businesses when they are establishing new improvement districts and business organizations that support growing and redeveloping commercial and mixed-use areas of the City.
- e. Define and identify under-utilized and under-performing commercial areas and develop tools to assist in the revitalization of the properties.
- f. Support small business growth through continued outreach efforts, educational opportunities, and funding assistance.
- g. Continue to cooperate and coordinate with business recruitment partners to attract new businesses to Lakewood and the Denver region.
- h. Continue the targeted use of economic incentives for projects that significantly enhance employment opportunities in Lakewood, particularly in Growth Areas identified in this chapter.
- i. Identify and evaluate opportunities to encourage additional investment in Lakewood.

**GOAL I-CS2**

**Market and brand Lakewood as a community in which to live, work, learn, and play.**

**ACTION STEPS**

- a. Develop a plan to market Lakewood as a strategic business and employment hub by working with existing business leaders and City officials to promote the City, highlight successes, encourage new investment, and direct investment to catalytic sites in the City.
- b. Evaluate the need for a marketing campaign to highlight Lakewood's unique position in the region with access to the mountains and to cultural and sporting events in Denver.
- c. Identify the resources necessary to support a strong and vibrant economic development program, including ways to leverage private sector contributions to enhance Lakewood's economic development efforts.

### GOAL I-CS3

**Enhance commercial and mixed-use districts to create quality working, living, shopping, and dining experiences.**

#### ACTION STEPS

- a. Annually monitor Lakewood's business and retail environment to evaluate the variety and diversity of business types within the city, as well as a variety of business scales and sizes.
- b. Identify opportunities to work with owners of older commercial and mixed-use properties to reinvigorate projects through reinvestment and redevelopment.
- c. Identify opportunities to promote commercial and mixed-use investment and reinvestment in the designated Growth Areas identified in this chapter.
- d. Enhance the vibrancy and attractiveness of commercial and mixed-use areas through the development of design guidelines for appropriate areas. Guidelines for high quality design and urban amenities for public and private development help attract and maintain employers and employees that will sustain economic growth.
- e. Develop criteria for the installation of public art within Growth Areas.
- f. Evaluate the need for additional reinvestment areas and support the Lakewood Reinvestment Authority in its efforts to facilitate redevelopment and reinvestment in designated urban renewal areas.
- g. Develop an educational campaign to ensure that property owners and tenants within commercial areas are educated about maintenance and other code enforcement issues. Proactively enforce codes as appropriate to ensure a better visual appearance of these areas.

### GOAL I-CS4

**Encourage visitors and Denver area residents to utilize Lakewood's lodging facilities, cultural amenities, and entertainment venues.**

#### ACTION STEPS

- a. Identify ways to encourage the development of new lodging, cultural, and entertainment venues in Lakewood to capture an increasing portion of the visitor economy.
- b. Develop a citywide way-finding program for important community and visitor destinations (Also see *Lakewood Moves*).
- c. Evaluate the need for a convention or event center facility to attract business meetings and regional and entertainment events.
- d. Identify ways to promote cultural and heritage tourism and assets.



**GOAL I-CS5**

**Cultivate an entrepreneurial and academic environment that fosters a growing local economy.**

**ACTION STEPS**

- a. Develop partnerships with Jeffco Public Schools, private secondary educational institutions, and higher educational facilities in the city to increase student interest in business creation and innovation, and encourage students to start businesses in Lakewood following graduation.
- b. Identify ways to encourage local businesses to hire talented students from local secondary and higher education facilities.
- c. Identify ways to attract business incubator, coworking, and other similar facilities to the city to foster and increase entrepreneurial activity within the community.

**GOAL I-CS6**

**Ensure that economic development tools remain flexible in a constantly changing economy.**

**ACTION STEPS**

- a. Identify and monitor important trends that will affect Lakewood's economy.
- b. Develop public-private partnerships to encourage the development of new tools and programs at the regional and state level that can be utilized in City efforts to attract desired businesses, such as technology and medical companies.
- c. Evaluate the City's development review process and identify opportunities to assist small business and streamline minor improvements to commercial sites.
- d. Publish economic development statistics and demographics on a regular basis and maintain up-to-date marketing material.
- e. Annually evaluate and improve the development approval process in order to best serve the interests of the City, and the business and development community.
- f. Identify ways to encourage private sector efforts to implement state-of-the-art technology, such as broadband internet, throughout the city to encourage new businesses and employees to locate in Lakewood.

**GOAL I-CS7**

**Continue to diversify Lakewood's economy to strengthen and stabilize the tax base and maintain viability through fluctuating economic cycles.**

**ACTION STEPS**

- a. Target and support business creation, retention, and attraction efforts for strategic industry clusters that are important to Lakewood.
- b. Develop a comprehensive slate of programs and outreach to foster the creation and growth of small businesses in the city.
- c. Evaluate and enhance programs for the pursuit and attraction of new and unique retailers and restaurants to Lakewood in order to enhance future sales tax revenue.

**GOAL I-CS8****Encourage development or redevelopment on brownfield properties.****ACTION STEPS**

- a. Identify possible remediation funding sources, including from the U.S. Environmental Protection Agency, and provide information to owners of brownfield properties.
- b. Create educational materials for property owners on options for mitigation and redevelopment of brownfield sites.



**Remediated Brownfield Site at Garrison Station**

**Financing Brownfield Cleanup**

The City of Lakewood continually seeks Brownfield grant opportunities offered by the U.S. Environmental Protection Agency (EPA) and aids in the administration of loan funds to help property owners clean brownfield properties. In addition to the loan fund, the Lakewood Environmental Assessment Support program can assist with funding environmental investigation costs for construction, renovation, transfer, or reuse of commercial or industrial property hindered by environmental issues.

# GROWTH AREAS

## Overview

Growth Areas are locations within the city that have been planned and zoned for higher density employment, retail and residential growth. These areas are found at strategic locations throughout Lakewood. Within the northern portion of the city, Growth Areas can be found along Colfax Avenue and the West Rail Line and at Denver West. Growth Areas are also located along Union Boulevard and at the Federal Center, Downtown Lakewood, and at Jewell Avenue and Wadsworth Boulevard in the central portion of the city. In the southern portion of the city, Growth Areas have been designated within Rooney Valley and surrounding the US 285 and Wadsworth Boulevard interchange, including Academy Park.

The Growth Areas are intended to accommodate the vast majority of the residential and employment increase anticipated for the City by 2035. These areas are generally intended for mixed-use, pedestrian-friendly projects with multi-story buildings and residential densities of 30 or more units per acre in core areas.

Growth Areas are located along major transportation corridors, with frequent rail and bus service in addition to automobile access. It is important that the transportation system maintain adequate capacity to accommodate the future growth intended for these areas. As growth occurs, the City will monitor the transportation system to make sure it supports the city's land use vision.

The principles of transit-oriented development should be applied to all of the Growth Areas within Lakewood, as access to frequent bus or rail transit exists in most of these locations. In those locations where transit access is limited, the principles should be applied in a way to encourage future bus or rail service or increases in existing service.

The City will strive to involve and coordinate any future planning efforts within the Growth Areas with established metropolitan districts and property owners. All planning efforts will need to reflect current and anticipated market and economic conditions.

The designated Growth Areas are indicated on Map 5-d on the following page.

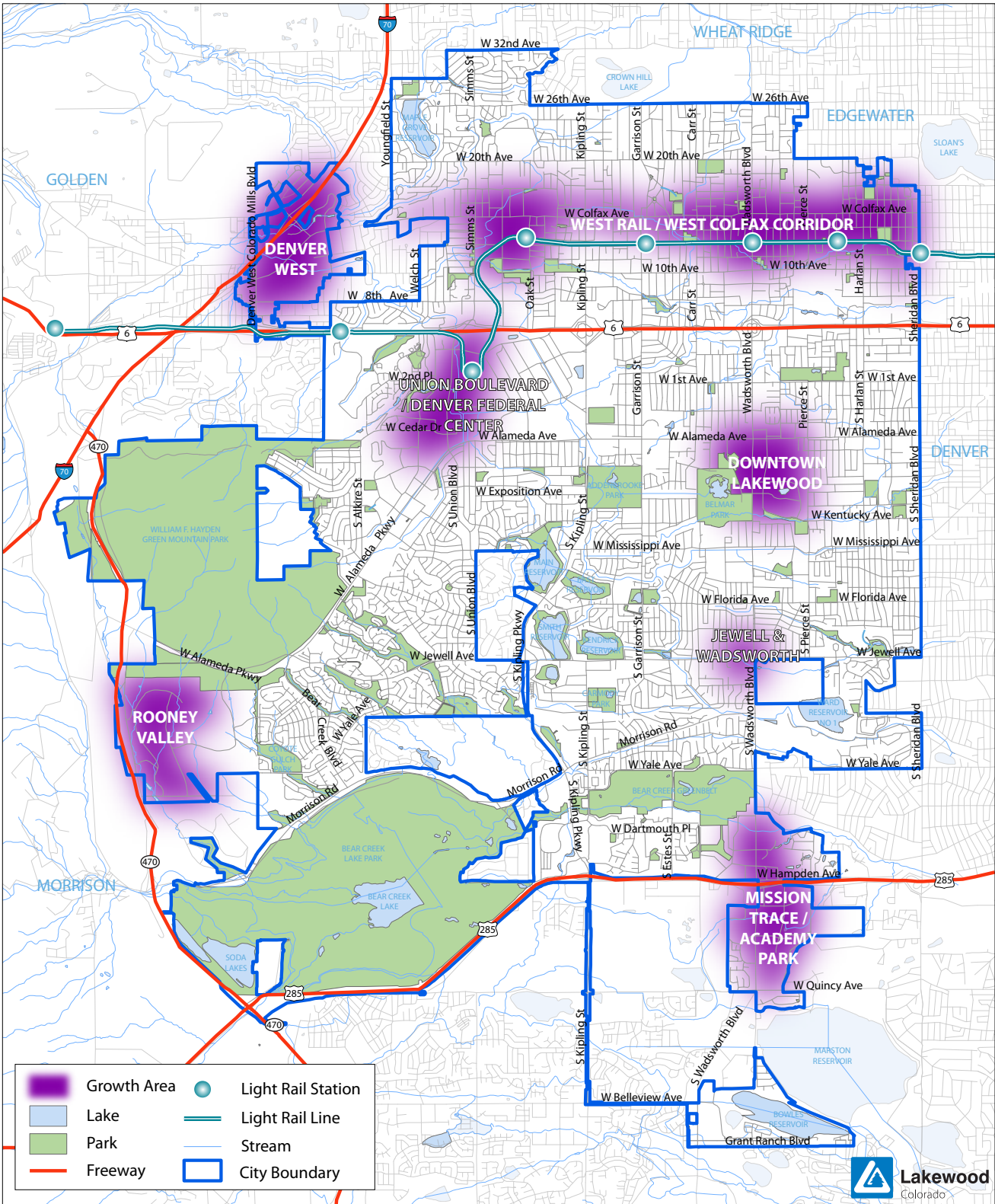
### Transit-Oriented Development Principles

Transit-oriented development, or TOD, is a land use development pattern that creates a walkable, vibrant, and mixed-use environment that is served by frequent, high quality transit service that connects the community to the rest of the region.

Transit-oriented development should be accessible and have a high level of alternative transportation connectivity with the surrounding community and region. TOD should also create place by including community gathering spaces and enclosing those spaces with buildings of high quality architecture. A mix of uses should always be part of a transit-oriented development, with an emphasis on multi-story, mixed-use buildings. Higher densities of residential and employment should be encouraged within TOD areas in order to support the high quality transit options.

Finally, and most importantly, transit-oriented development should be pedestrian, bicycle, and transit friendly, with far less emphasis placed on moving people via the automobile. To the greatest extent possible, the automobile should be de-emphasized as part of the development.





### Map 5-d

## Growth Areas



# Colfax Avenue & West Rail Line Growth Area

The Colfax Avenue and West Rail Line Growth Area encompasses Colfax Avenue between the city's eastern boundary with Denver to Simms Street on the west. The Growth Area also encompasses five of the seven rail stations located in the city.

Colfax Avenue has been an important street for economic growth since the early 1900s, long before Lakewood was incorporated. The focus of Colfax Avenue until the late 1960s had been on moving automobiles through the area to the mountains and into Denver. Colfax Avenue, also known as US 40, was known in the 1940s and 1950s for the many auto-oriented motels, restaurants, and businesses that were built to accommodate America's passion of traveling by automobile. However, as alternate travel routes to Colfax Avenue were built, such as Interstate 70 and US 6, travelers began to find other locations to stay, shop and eat. The use of motels began to decline and many ultimately became weekly rentals. Many businesses along the street transitioned from visitor-oriented restaurants to fast food locations and other businesses such as liquor stores, pawn shops and tattoo parlors. Some buildings were torn down and the properties remain as vacant or under-utilized lots.

However, in the past decade, Colfax Avenue has seen a resurgence in development activity, with completion of the Creekside Center at Wadsworth Boulevard and the opening of a Home Depot at Teller Street. Reinvestment has also started to occur near the Lamar Street light rail station area with new art-related and creative industry businesses.

The introduction of rail service by RTD just two blocks south of Colfax Avenue has also enhanced the profile of the corridor. The areas surrounding the five rail stations have started to see interest in reinvestment and new development.

Redevelopment along Colfax Avenue and surrounding the rail stations will provide the City with numerous opportunities to increase employment, business, and residential densities while preserving and serving the surrounding stable neighborhoods.



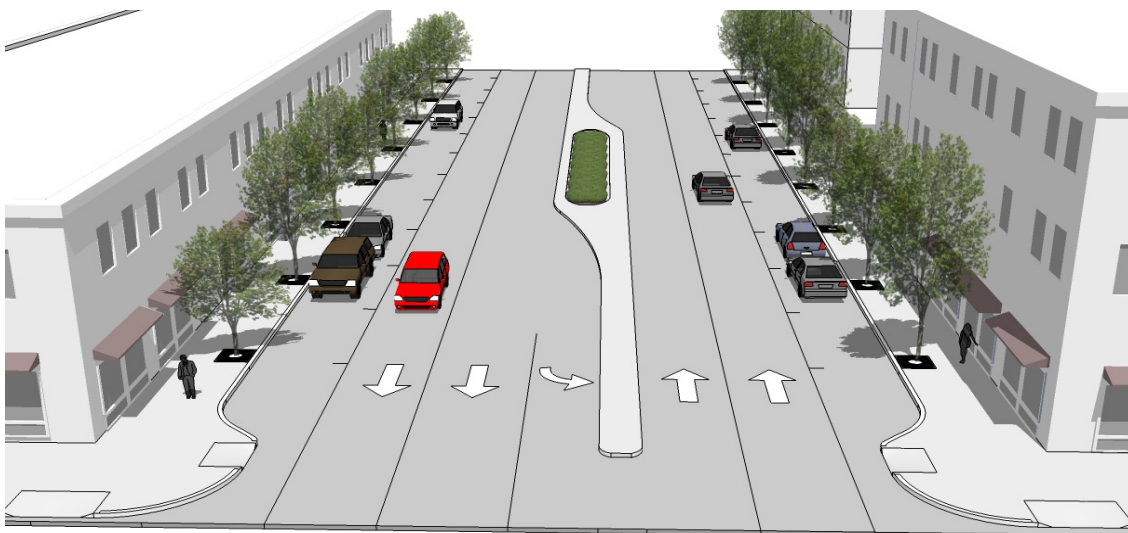


## GOAL I-GA1

**Support community efforts to transition Colfax Avenue from a suburban arterial roadway to a complete urban street and primary economic corridor.**

### ACTION STEPS

- Support the Lakewood West Colfax BID, community organizations, and the Colorado Department of Transportation (CDOT) in efforts to develop a plan for Colfax Avenue that meets an urban Complete Street standard by evaluating options for on-street parking lanes, reduced vehicular access, a complete sidewalk system, pedestrian amenities, and sustainable elements.
- Develop a plan and funding mechanism to address the drainage issues along Colfax Avenue that limit the economic development potential and complete street transformation efforts.
- Evaluate potential incentives to encourage property owners to include a mix of residential and commercial uses along Colfax Avenue as redevelopment and reinvestment occurs.
- Develop criteria for providing incentives for catalytic mixed-use projects along the Colfax Avenue corridor.
- Evaluate the effectiveness of the current zoning along Colfax Avenue to ensure that redevelopment projects meet the design and dimensional requirements. Consider modification of the Zoning Ordinance if issues are identified.
- Partner with the Lakewood West Colfax BID, 40 West Arts District, and WCCA to hold events along Colfax Avenue that will stimulate economic growth along the corridor, such as Lakewood on Parade, the Colorado Colfax Marathon and Inspire Arts Week.
- Encourage historic preservation and promote the associated preservation tax credits, where feasible, as an economic development tool for the Colfax Avenue corridor.
- Regularly evaluate the effectiveness of branding and marketing campaigns for the corridor.



**Conceptual illustration of Colfax Avenue with on-street parking and improved sidewalks**



## GOAL I-GA2

**Transform the Sheridan Boulevard West Rail Station Area into a mixed-use neighborhood center with a predominately residential focus in accordance with the Growth Area Land Use Map and transit-oriented development principles.**

### ACTION STEPS

- Identify routes and funding for the development of a bike and pedestrian connection between Lakewood Gulch in Denver, Mountair Park and Harlan Street to complete the West Line trail system.
- Develop design specifications and implementation policies to ensure that a wide sidewalk and amenity zone is provided along Sheridan Boulevard within the station area with new or redevelopment projects. A minimum width of 17 feet should be provided for the sidewalk and amenity zone areas.
- Identify tools to encourage projects within the Station Core and Mixed-Use Commercial areas to maximize the densities and building heights allowed by zoning.
- Continue to implement, and annually evaluate, the recommendations of the *20 Minute TOD Neighborhood Implementation Strategy*.

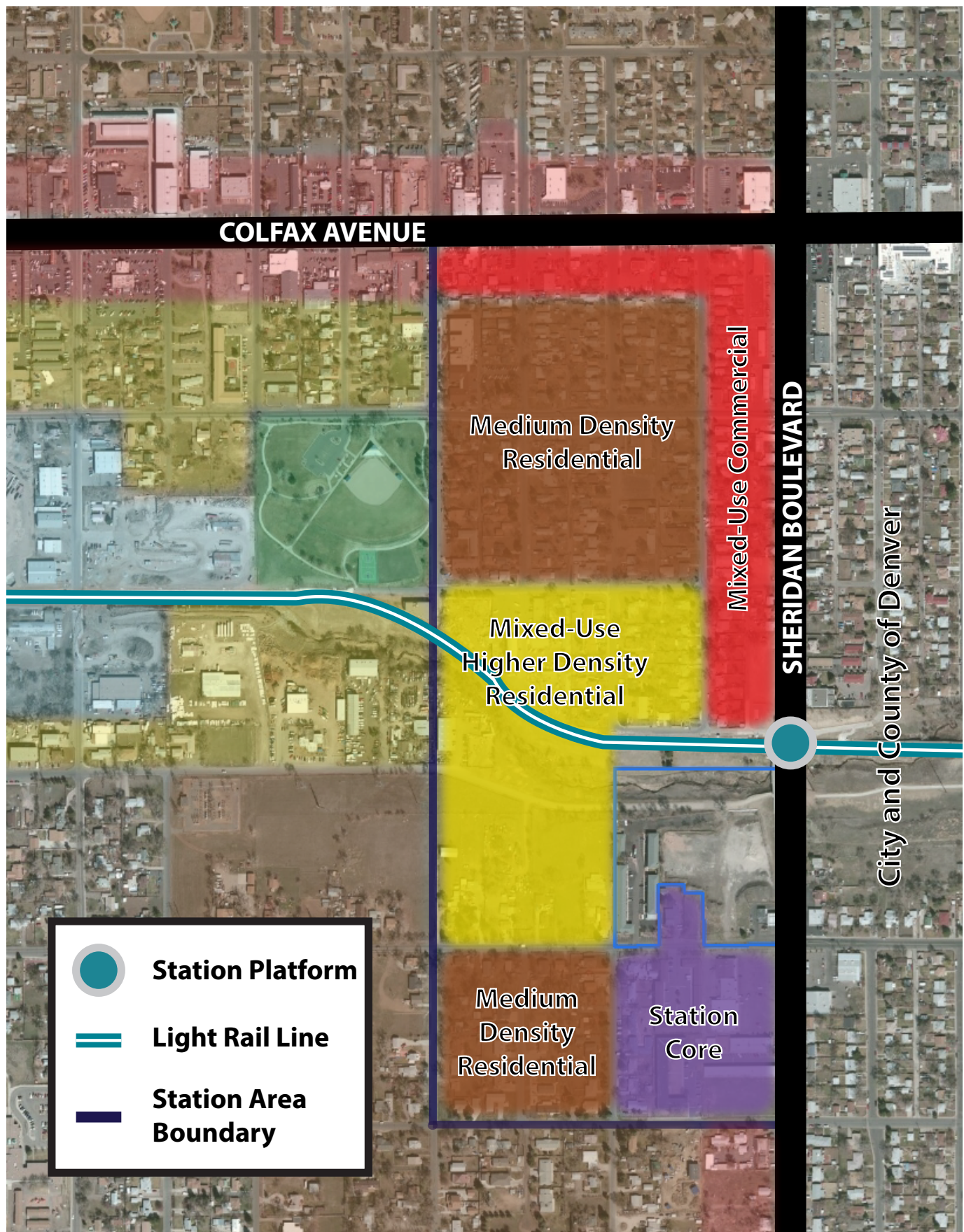


### 20 Minute TOD Neighborhood Implementation Strategy

What is a 20-minute neighborhood? It is a neighborhood where residents have safe and convenient access by walking, bicycling, or transit to many of the places and services they use daily, including local markets and other neighborhood serving businesses, restaurants, schools, and parks within a half-mile or 20-minute walking distance, and within a short distance of a transit hub. In short, it's a neighborhood where residents can have a high quality of life without needing to own a personal vehicle – either because they can't afford to or because they choose not to.

Living in a 20-minute neighborhood has many benefits. Residents have more personal disposable income when vehicle ownership and maintenance is not a factor and also have the potential for improved personal health when shopping and errands can be completed within a short walk or bike ride. A 20-minute neighborhood is also supportive of growing a local economy where residents in the area can easily walk or bike to meet their daily needs and support local businesses.

The Sheridan Boulevard West Rail station area was chosen as the first location in the city to test these principles through the strategy. More detail regarding the strategy can be found on the City's website.



Map 5-e

Sheridan Station Growth Area Land Use Map



## Growth Area Land Use Designations

**Station Core Areas** - These areas are intended to include projects with a mix of retail, office, and multifamily residential uses with minimum building heights of three stories. Maximum heights vary between 90 feet at the Oak Street Station, to 120 feet at the Sheridan and Lakewood-Wadsworth Stations and 180 feet along the Union Boulevard corridor. The zoning designation related to these areas is M-C-T.

**Neighborhood Station Core Areas** - These areas are intended to allow projects with a mix of retail, office, and multifamily residential uses with minimum building heights of two stories and a maximum height of 60 feet. The zoning designation related to these areas is M-N-T.

**Mixed-Use Commercial Areas** - These areas should include commercial and office uses, with residential as a secondary component. Building heights should generally range between two and six stories. These areas are focused on Colfax Avenue and the major arterial streets within the station areas. The zoning designations related to these areas are M-G-T, M-G-U and M-N-U. In limited areas where suburban style development is allowed along Colfax Avenue, M-G-S has been designated.

**Research and Development Areas** - These areas are intended to allow existing and new research and technology users to develop high quality employment districts within close proximity to light rail stations. Campus-style development is encouraged. Building heights should range between one and four stories. The zoning designation related to these areas is LI-RD.

**Mixed-Use Office** - These areas are intended to allow for mid- and high-rise office development, as well as retail and services uses. The zoning designations related to these areas are M-E-U, M-G-T, and M-G-U.

**Mixed-Use Higher Density Residential Areas** - These areas should be focused primarily on residential projects with a minimum density of 30 units per acre. Secondary retail and office uses should be provided as part of a project, where appropriate, to support the residential development. The zoning designation related to these areas is M-R-T.

**Medium Density Residential Areas** - These areas are primarily intended for single-family attached and multifamily residential development, although small ancillary retail and office uses are also allowed. Projects should have densities between five and 30 units per acre. The zoning designations related to these areas are M-R-U and M-R-S and R-MF.

**Lower Density Residential Areas** - These areas are intended to allow for the preservation of existing lower density neighborhoods, while allowing for small-scale redevelopment. Projects are allowed to have a density of up to 10 units per acre. The zoning designations related to these areas are R-2 and R-1-6.



## GOAL I-GA3

**Transition the Lamar Street West Rail Station Area into a dynamic community that creates place, connects community, and celebrates history in accordance with the Growth Area Land Use Map and transit-oriented development principles.**

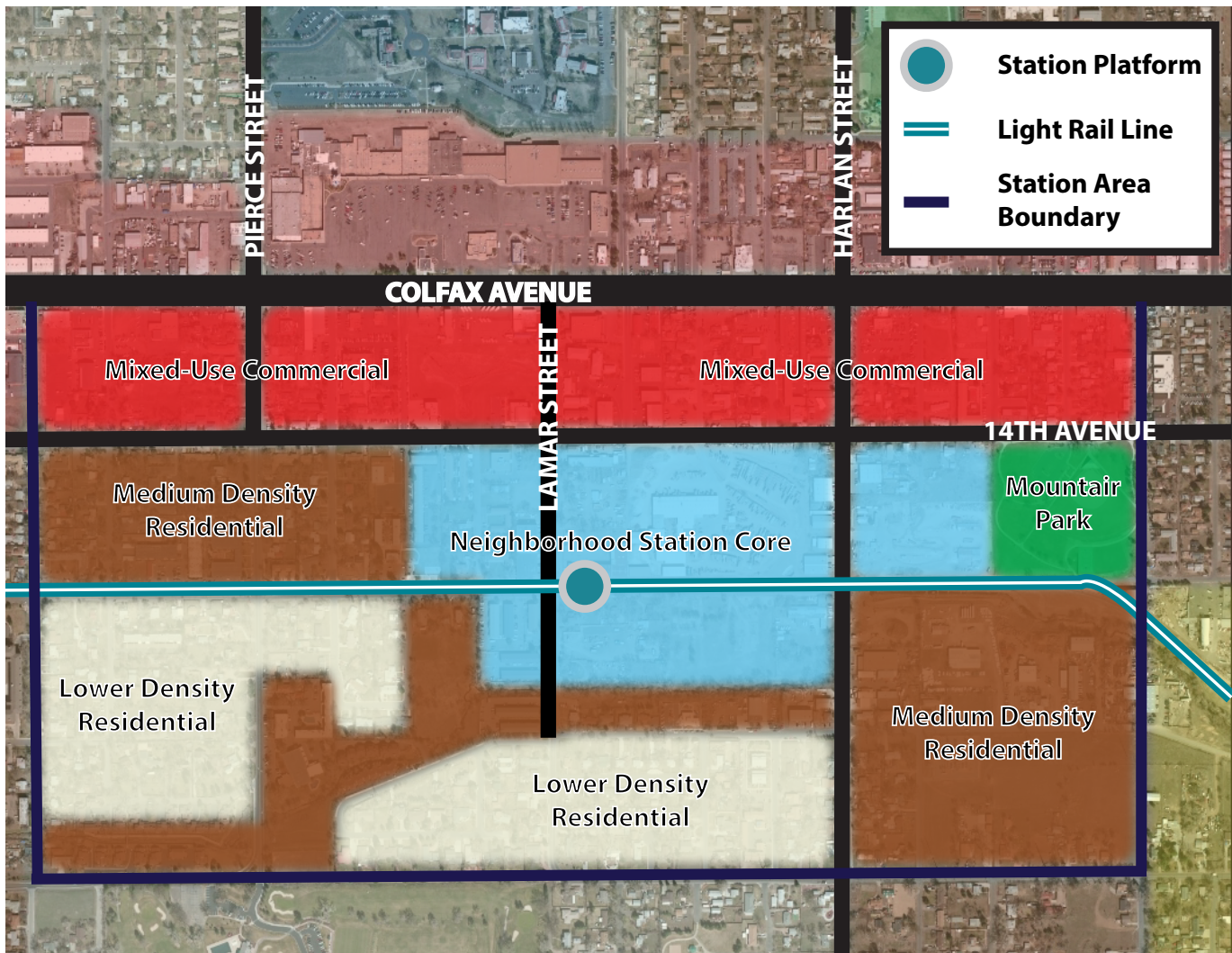
### ACTION STEPS

- a. Continue to implement, and annually evaluate, the recommendations of the *40 West Arts District Urban Design and Mobility Concepts Plan*.
- b. Develop a Complete Street plan for 14th Avenue within the station area that creates a vibrant, active and pedestrian-friendly atmosphere and that includes wider sidewalks, bike lanes and pedestrian amenities between the station platform and Colfax Avenue.
- c. Identify tools to encourage preservation and revitalization of existing buildings where feasible to support the eclectic feel of the station area.
- d. Develop design specifications and identify locations where bricks and brick pavers can be installed within public and private streetscapes to reflect the industrial history of the area. Encourage the utilization of brick on building facades as well.
- e. Evaluate and identify redevelopment opportunities for the Lamar Station Plaza shopping center and ways to connect the Rocky Mountain College of Art + Design (RMCAD) to Colfax Avenue and the rail station.
- f. Develop tools to encourage a balanced mix of affordable, workforce and market-rate residential projects within the station area.
- g. Identify opportunities to recognize the history and contribution of the Jewish Consumptive Relief Society (JCIRS) within the station area.

### 40 West Arts District Urban Design and Mobility Concepts Plan

The Urban Design and Mobility Concepts study outlines ideas for revitalizing the area surrounding the Lamar Street West Rail station by forming a new arts district and providing improvements to the public realm in the form of streetscape improvements, lighting, signage and way finding and the integration of arts throughout the area. The document contains specific recommendations focused on urban design and mobility.

The vision put forth in the document is based on the following concepts: celebrating the Lamar Station as a central feature; enhancing the Lamar Street streetscape experience from the station to West Colfax Avenue and connecting to the expanded Rocky Mountain College of Art + Design (RMCAD) campus in the Lamar Station Plaza shopping center; redefining West Colfax Avenue, generally between Pierce and Harlan Streets, as a gateway to the arts district with enhanced pedestrian crossings, signage and streetscape improvements; implementing lighting, signage and graphics, including wayfinding maps and arts district information kiosks, to establish a creative and unified identity and branding program; and developing the Arts Loop as a themed multi-use bicycle and pedestrian loop throughout the area, connecting key parks and highlighting the arts.



Map 5-f

Lamar Station Growth Area Land Use Map



Rendering of potential development looking northeast from Lamar Street and 13th Avenue

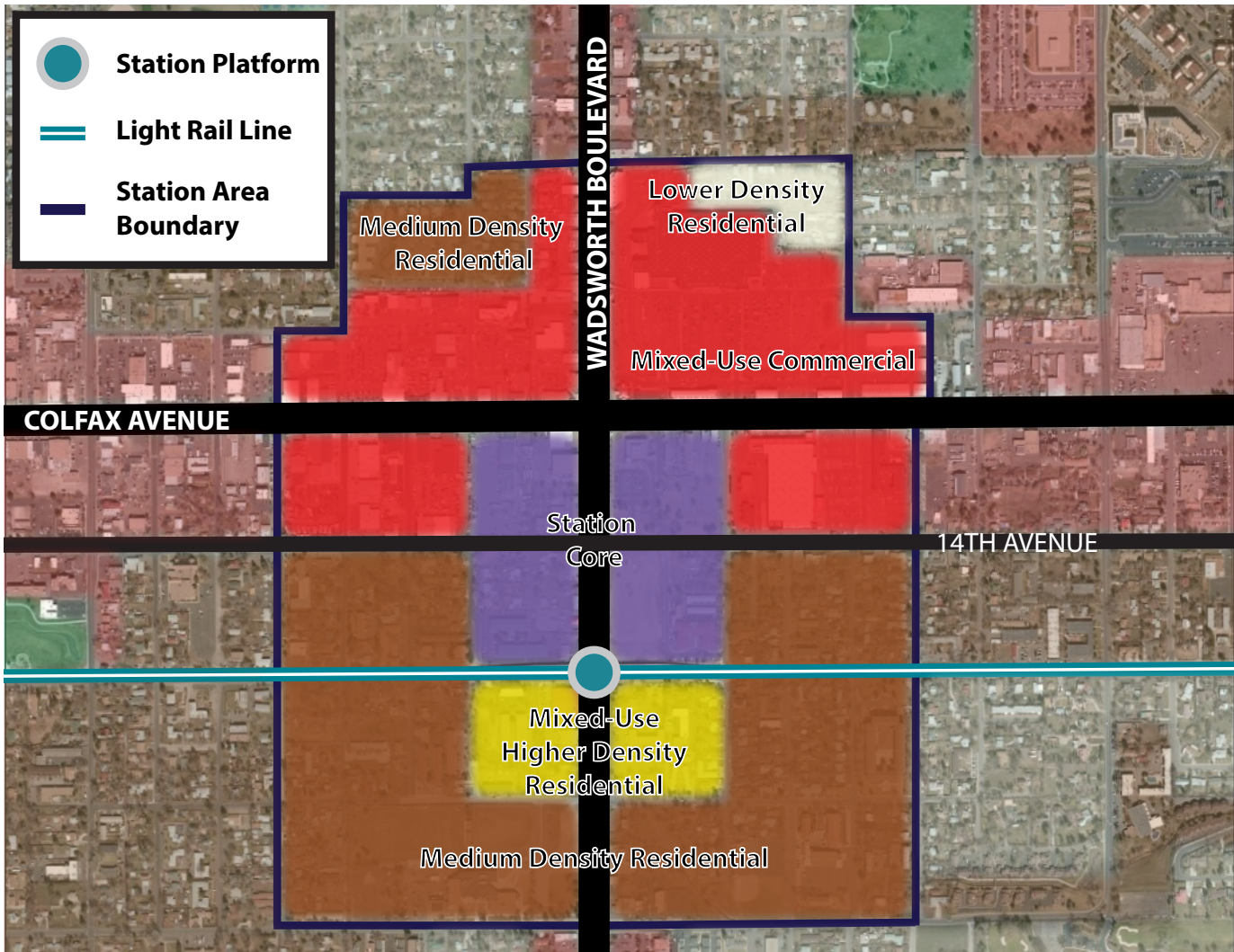
## GOAL I-GA4

**Transform the Lakewood•Wadsworth Boulevard West Rail Station Area into a mixed-use urban village with lively commercial, employment, and residential districts in accordance with the Growth Area Land Use Map and transit-oriented development principles.**

### ACTION STEPS

- a. Develop design specifications and implementation policies to ensure that a wide sidewalk and amenity zone is provided along Wadsworth Boulevard within the station area with new or redevelopment projects. A minimum width of 17 feet should be provided for the sidewalk and amenity zone areas.
- b. Conduct an area assessment, and identify opportunities and tools for the consolidation of smaller individual properties that are in close proximity to the station to allow for the development of catalytic mixed-use projects.
- c. Develop a Complete Street plan for 14th Avenue through the station area that creates a pedestrian-friendly environment as an alternative to Colfax Avenue for smaller scale retail and office uses.
- d. Identify tools to encourage projects within the Station Core and Mixed-Use Commercial areas to maximize the densities and building heights allowed by zoning.
- e. Develop an agreement with RTD to allow for the use of the Lakewood•Wadsworth station street level plaza as a community gathering place and location for special events.





**Map 5-g** Lakewood-Wadsworth Station Growth Area Land Use Map



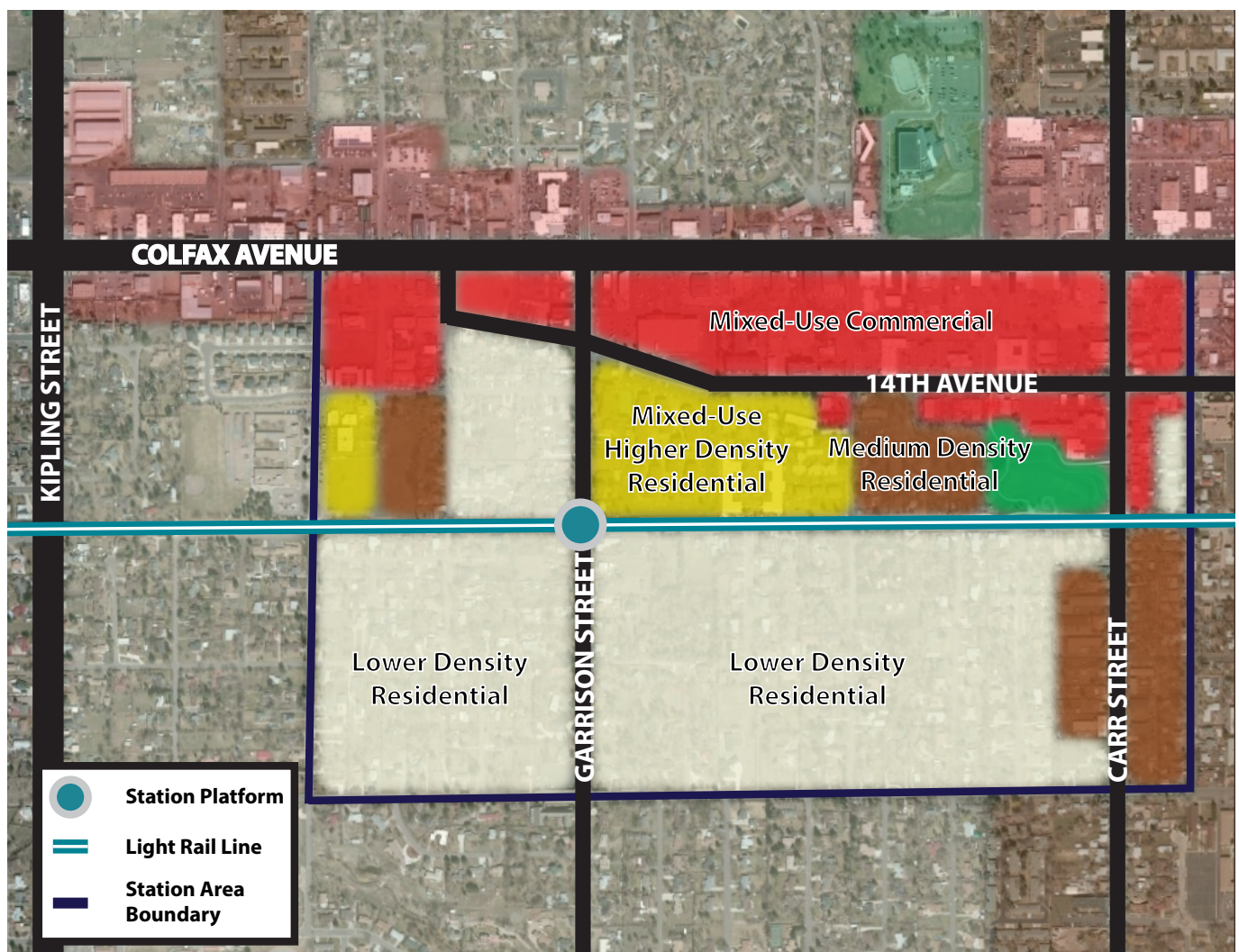


## GOAL I-GA5

**Maintain the Garrison Street West Rail Station Area as a smaller-scale, neighborhood-oriented district in accordance with the Growth Area Land Use Map and transit-oriented development principles.**

### ACTION STEPS

- Protect the integrity of the single-family detached residential neighborhoods located south of 13th Avenue and west of Garrison Street by maintaining single-family (R-1) zoning.
- Develop design guidelines for new development north of the station so that sites and buildings will be compatible and complementary to the surrounding uses and reflect a neighborhood-oriented scale.
- Develop a Complete Street plan for 14th Avenue through the station area and redesign the street to reflect a neighborhood-oriented retail and office scale.



**Map 5-h**

**Garrison Station Growth Area Land Use Map**



**Rendering of potential development along 14th Avenue in the Garrison Station Area**



## GOAL I-GA6

**Transform the Oak Street West Rail Station Area into a mixed-use center, with an emphasis on research and development and retail opportunities in accordance with the Growth Area Land Use Map and transit-oriented development principles.**

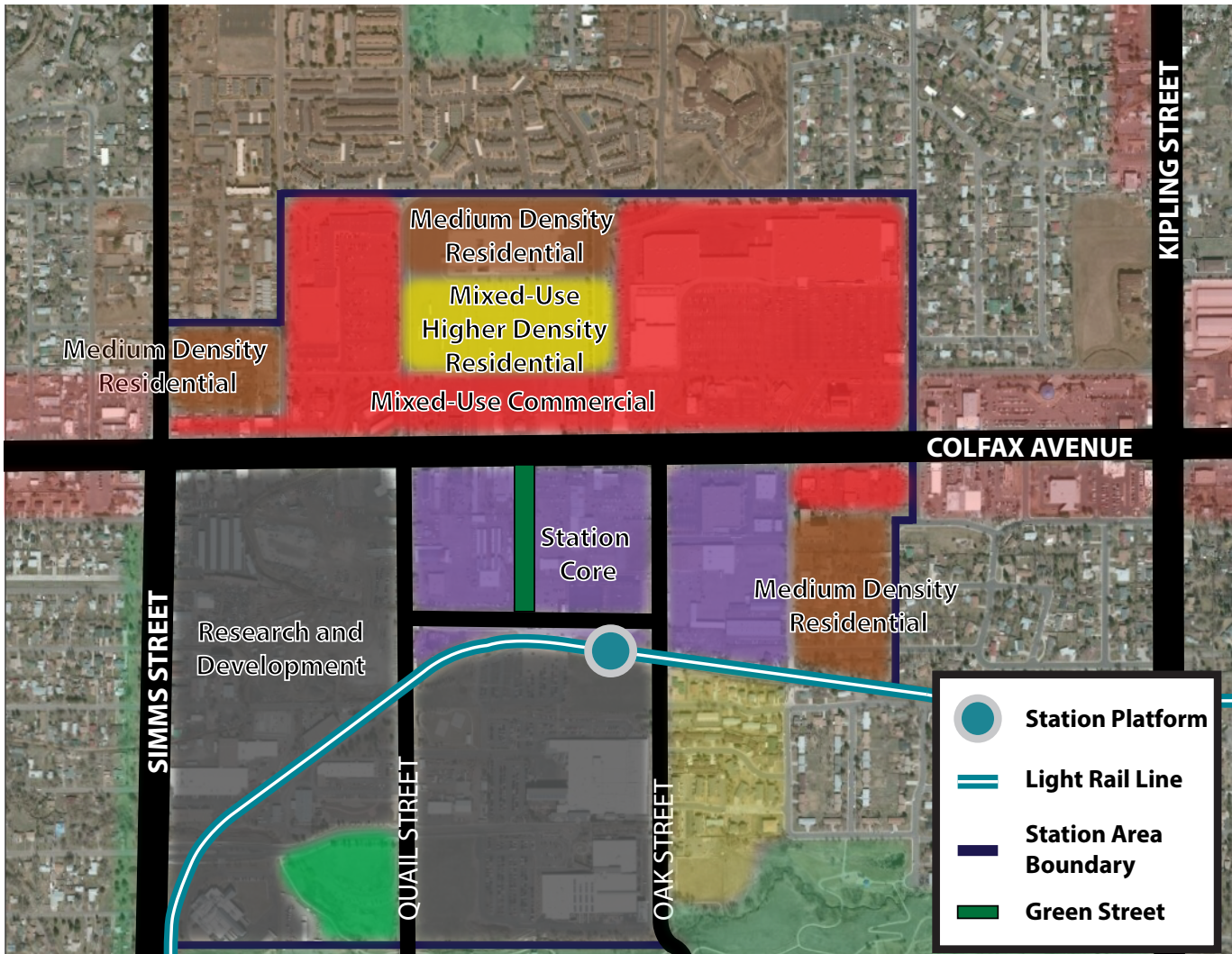
### ACTION STEPS

- a. Develop a plan for the 18-acre property under RTD ownership east of Simms Street and south of Colfax Avenue that identifies access, utility needs, and appropriate land uses.
- b. Develop a specific land use plan for the Station Core area that evaluates mixed-use development opportunities for the existing RTD surface parking facility adjacent to the station platform, the automobile dealership property south of Colfax Avenue, and other surrounding properties. The plan should also include the development of a green street, or tree lined parkway, concept between the rail station and Colfax Avenue as identified on the Growth Area Land Use Map.
- c. Develop tools to encourage projects within the Station Core and Mixed-Use Commercial areas to maximize the densities and building heights allowed by zoning.
- d. Identify and evaluate mixed-use redevelopment opportunities for the Westland Shopping Center site.
- e. Identify a route and funding opportunities to extend the West Rail Line bike and pedestrian trail west through the station area.
- f. Evaluate opportunities to partner with Terumo BCT to create a vibrant corporate campus and quality pedestrian and bicycle connections to the rail station.
- g. Research potential funding opportunities for the development of an interurban transit museum and display building for the Denver and Intermountain Interurban Car #25.

### Denver and Intermountain Interurban Car #25

Denver once had an extensive electric rail transit system that included over 250 miles of city tracks and 40 miles of high-speed interurban railcars connecting Denver with Golden and Boulder. Thousands of riders used the system to visit downtown Denver and its venues for shopping, going to work and school, seeing friends, and for recreation. Rich in local history, Car #25 was built by Denver's own Woeber Carriage Company and placed into service on February 17, 1911. Car #25 was one of seven cars that provided passenger service on the Denver & Intermountain Interurban's 13-mile Denver-Lakewood-Golden line from 1911 to 1950. With the proliferation of the automobile and a new bus system, rail transit service was abandoned in 1950.

Car #25 is the last remaining, completely intact, electric railway car that once served the Denver area. As an exquisitely restored piece of history, Car #25 is listed on both the State Register of Historic Properties and the National Register of Historic Places. Car #25 is currently stored at the Denver Federal Center with limited public access.



Map 5-i

Oak Station Growth Area Land Use Map





# Union Boulevard & Federal Center Growth Area

The Union Boulevard and Federal Center Growth Area includes the Union Boulevard corridor between 6th Avenue and Alameda Avenue and the Denver Federal Center Campus.

The Denver Federal Center campus encompasses approximately 600 acres and currently includes 4.1 million square feet of office and research space in approximately 44 buildings with over 6,000 employees within 28 federal agencies. The Denver Federal Center was originally acquired by the United States government in 1941 and was used as an arms manufacturing facility during World War II. Since that time, the campus has been converted to nonmilitary uses. In 2007 the General Services Administration (GSA) received approval of an Environmental Impact Statement (Master Site Plan) for the property. The plan is detailed on page 5-36 of this chapter.

This growth area also includes the St. Anthony Medical Campus and the associated OrthoColorado Hospital and medical office buildings that provide a major anchor for the area. The campus plans allow for expansion of both the medical facility and the addition of other medical office buildings.

The Union Boulevard corridor contains several million square feet of office space, as well as many hospitality and retail facilities. The corridor is one of the premier office locations within the western portion of the Denver region. The corridor, however, remains under-developed with numerous surface parking lots and older industrial and warehouse buildings adjacent to the St. Anthony Medical Campus.

A number of redevelopment and infill opportunities exist within the Union Boulevard and Federal Center Growth Area. Those opportunities can be found at older retail and office properties along the Union Boulevard corridor and on the Denver Federal Center campus, particularly within the Federal Center Station Neighborhood in the northwest corner of the campus. Additional retail, office, and residential density can be accommodated within this growth area.





## GOAL I-GA7

**Transform the areas along Union Boulevard and along the western portion of the Denver Federal Center into a high-density mixed-use urban corridor in accordance with the Growth Area Land Use Map and transit-oriented development principles.**

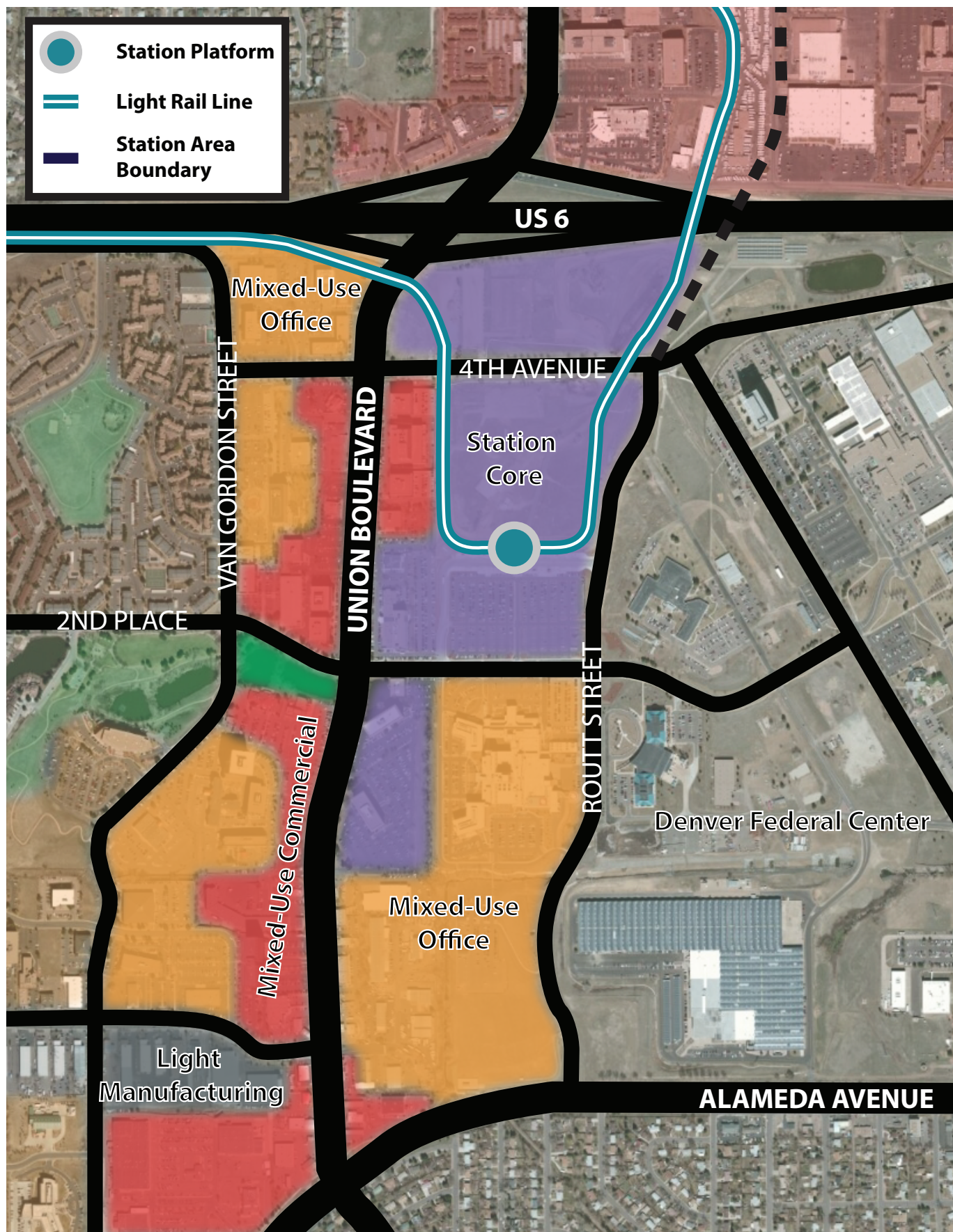
### ACTION STEPS

- a. Develop tools to encourage projects within the Station Core and Mixed-Use Commercial areas to maximize the densities and building heights allowed by zoning.
- b. Continue to implement, and annually evaluate, the recommendations of the *Union Boulevard Corridor/Federal Center Connectivity Plan* and the *Union Boulevard Corridor Urban Design Study*.
- c. Complete the acquisition of the northwestern corner of the Federal Center property and, with RTD, develop a plan to create a high density, mixed use development on the acquisition parcel and the RTD Park 'n Ride property.
- d. Evaluate potential tools, including a business improvement district, to assist with economic development and infrastructure improvements within the Growth Area.
- e. Evaluate opportunities to partner with Centura Health to facilitate continued expansion of the St. Anthony Hospital campus and development of necessary medical office buildings.

### Union Boulevard Corridor/Federal Center Connectivity and Urban Design Plan and Union Boulevard Corridor Urban Design Study

The *Union Boulevard Corridor/Federal Center Connectivity and Urban Design Plan* contains an evaluation of the corridor as it exists and recommendations to improve the overall multi-modal functionality of the area and ways to improve the pedestrian environment. The plan contains 11 recommendations for improvements, such as improving pedestrian access between the West Rail Line Federal Center Station and Union Boulevard; improving pedestrian and bike crossings of Union Boulevard; developing a bicycle system parallel to Union Boulevard; increasing transit accessibility within the plan area and to the area from other parts of the city; and suggestions for consistent pedestrian amenities in the area. The study identifies the difficulties with existing connectivity in the area and recommends both short-term actions that can be accomplished by the City and long-term actions that will work in conjunction with private redevelopment as it occurs along the corridor.

The *Union Boulevard Corridor Urban Design Plan* was developed to refine the vision for the area, help establish a sense of place, and create a consistent and identifiable character for the Union Boulevard Corridor. The plan also helps advance recommendations in several other adopted plans, including the 2003 Lakewood Comprehensive Plan and the Federal Center/Union Boulevard Corridor Connectivity Plan. The plan provides recommendations related to the design and location of future public and private investments such as streetscape improvements, sidewalks and pedestrian connections, benches, lighting, and directional signage. The plan identifies opportunities to create a more walkable and attractive urban district with strong connections to the Federal Center transit facility. The plan also provides a visual summary of how the corridor could begin to evolve as properties redevelop to higher density, mixed-use development.



Map 5-j

Union Boulevard &amp; Federal Center Growth Area Land Use Map

## **Denver Federal Center Environmental Impact Statement (Master Site Plan)**

The Denver Federal Center previously occupied a square mile of land in the central-western portion of Lakewood. The Federal Center was acquired by the United States government in 1941 for use as a munitions factory during World War II. The center now houses dozens of federal agencies, with approximately 6,000 people employed on the site.

The property was annexed by the City in 2007, in conjunction with the development of St. Anthony Hospital and the RTD Federal Center Station which are located on property formerly part of the Federal Center. A new master plan for the property was also developed in 2007 by the General Services Administration, which oversees the center.

The Master Site Plan for the Federal Center replaced a plan developed in 1997 and addressed the many opportunities for both public and private development on the site. An Environmental Impact Statement evaluated the proposed Master Site Plan alternatives and identified the environmental effects associated with implementing the proposed plan. The plan was approved through a Record of Decision in 2007.

The Federal Center currently has approximately 4.1 million square feet of office and research space in over 44 buildings. The Master Site Plan indicates that 2.8 million square feet of existing space will be demolished and replaced by 3.6 million square feet of new development. Additionally, 1,400 residential units are proposed to be developed, as well as two formal open space and park areas.

The defining characteristic of the Master Site Plan is the central quad planned for the core of the Federal Center property. Enhanced streetscapes throughout the campus will encourage workers to walk to and from transit and into adjacent districts. A new, mixed use center around the quad will be the heart of the campus and will be woven into the fabric of the surrounding neighborhoods and commercial districts via roadway and land use connections. The quad will be surrounded by complementary office buildings, including secure federal buildings, non-secure federal buildings and research buildings. Map 5-k on page 5-37 indicates the various districts.

Surrounding the quad will be a mixed-use office center district in the northwest corner of the Federal Center property that permits residential, office, research and development, and retail uses; a mixed-use core just to the east of the RTD Federal Center Station; a research and development district to the east of the St. Anthony Hospital campus; a federal campus district on the eastern two-thirds of the site; and a neighborhood retail district near Oak Street and Alameda Avenue.

As part of the master planning effort, the following vision statement was developed:

“This world-class federal campus integrates a mix of uses that complement the larger community. The campus serves as a model for innovative partnerships, enhancing value for taxpayers, and the efficiency and longevity of federal operations. In meeting the federal mission, the campus embraces sustainability, design excellence, and greater accessibility for the public. It serves as a prominent and progressive community hub, capitalizing on the availability of public transit and services. The campus reflects a renewed commitment to our collective future.”

Additional information about the Denver Federal Center and the Master Site Plan can be found on the General Services Administration website.

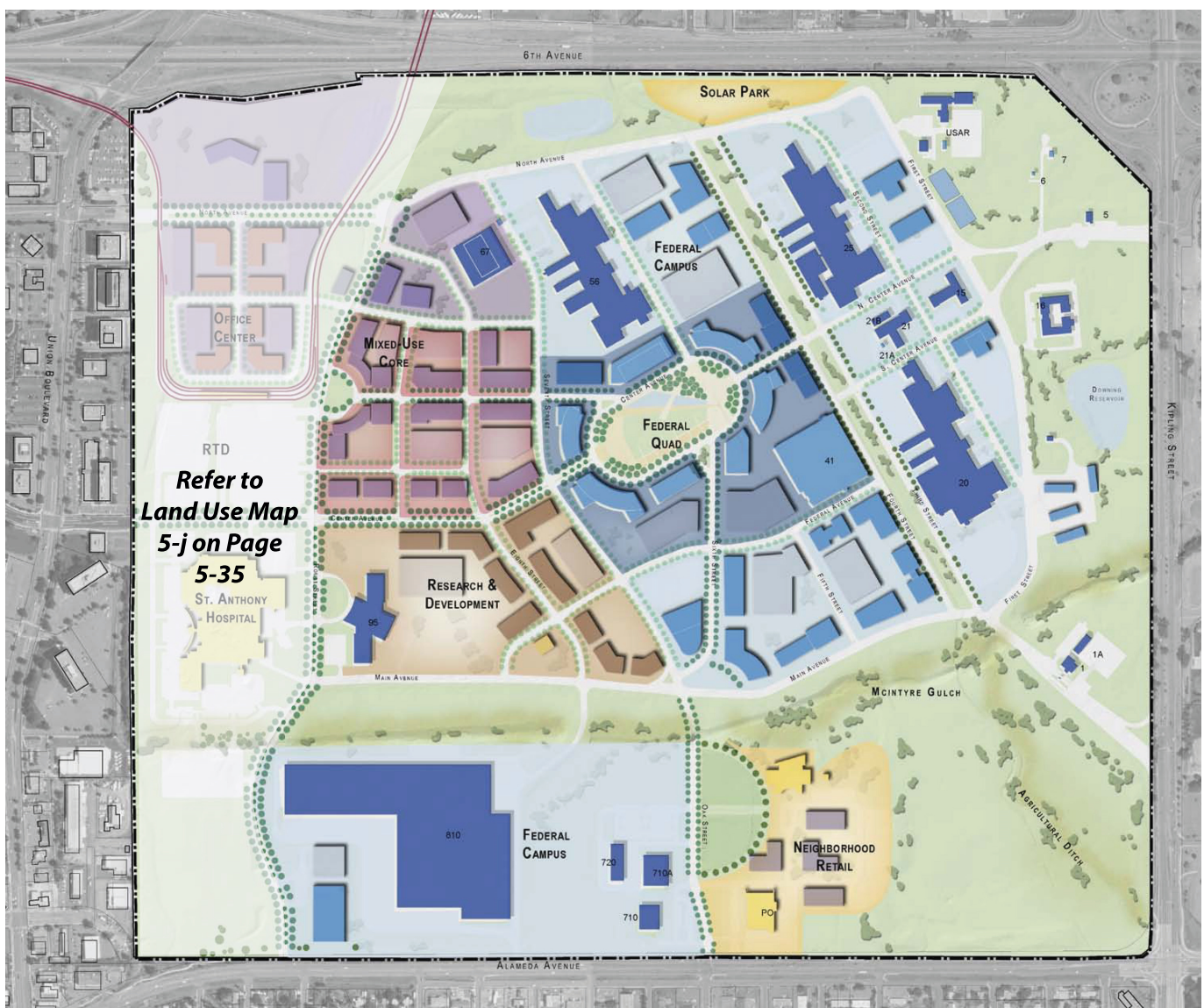


## GOAL I-GA8

Ensure a strong working relationship with the General Services Administration (GSA) and other Federal Government agencies to encourage increased employment and continued high quality development on the Denver Federal Center property.

### ACTION STEPS

- Maintain communications with the GSA and other federal government agencies in order to encourage increased employment on and near the Denver Federal Center campus.
- Assist, through City staff support, with the implementation of the development scheme for the Denver Federal Center as identified in the *Final Environmental Impact Statement* created by the GSA.



Map 5-k

Denver Federal Center Land Use Master Plan Map

# Downtown Lakewood Growth Area

Downtown Lakewood has changed significantly over the past two decades. A former regional mall has been redeveloped as Belmar, an award winning mixed-use development. Vacant land west of Wadsworth Boulevard has been replaced by Lakewood City Commons and the City's Civic Center complex. Downtown is a vibrant and changing area with over two million square feet of restaurant, retail, and commercial space supported by a residential base of over 4,600 people within the downtown area boundary.

The downtown area is also anchored by one of the city's largest parks, Belmar Park. The park includes active and passive spaces, as well as the Lakewood Heritage Center, with the 20th Century Museum, historic buildings, and a festival area. Belmar Plaza and Belmar Square also provide open space for downtown residents and visitors.

Downtown is also home to some of the community's biggest events, such as the Cider Days celebration and Belmar's Festival Italiano.

Although the downtown area has been transformed in recent years, there are still opportunities to increase residential and employment densities. Many of the properties north of Alameda Avenue are ripe for reinvestment and redevelopment. The Lakewood City Commons, while a vibrant retail center, should be activated with additional employment and residential uses.

A vibrant and growing downtown area is an important asset for the City of Lakewood.



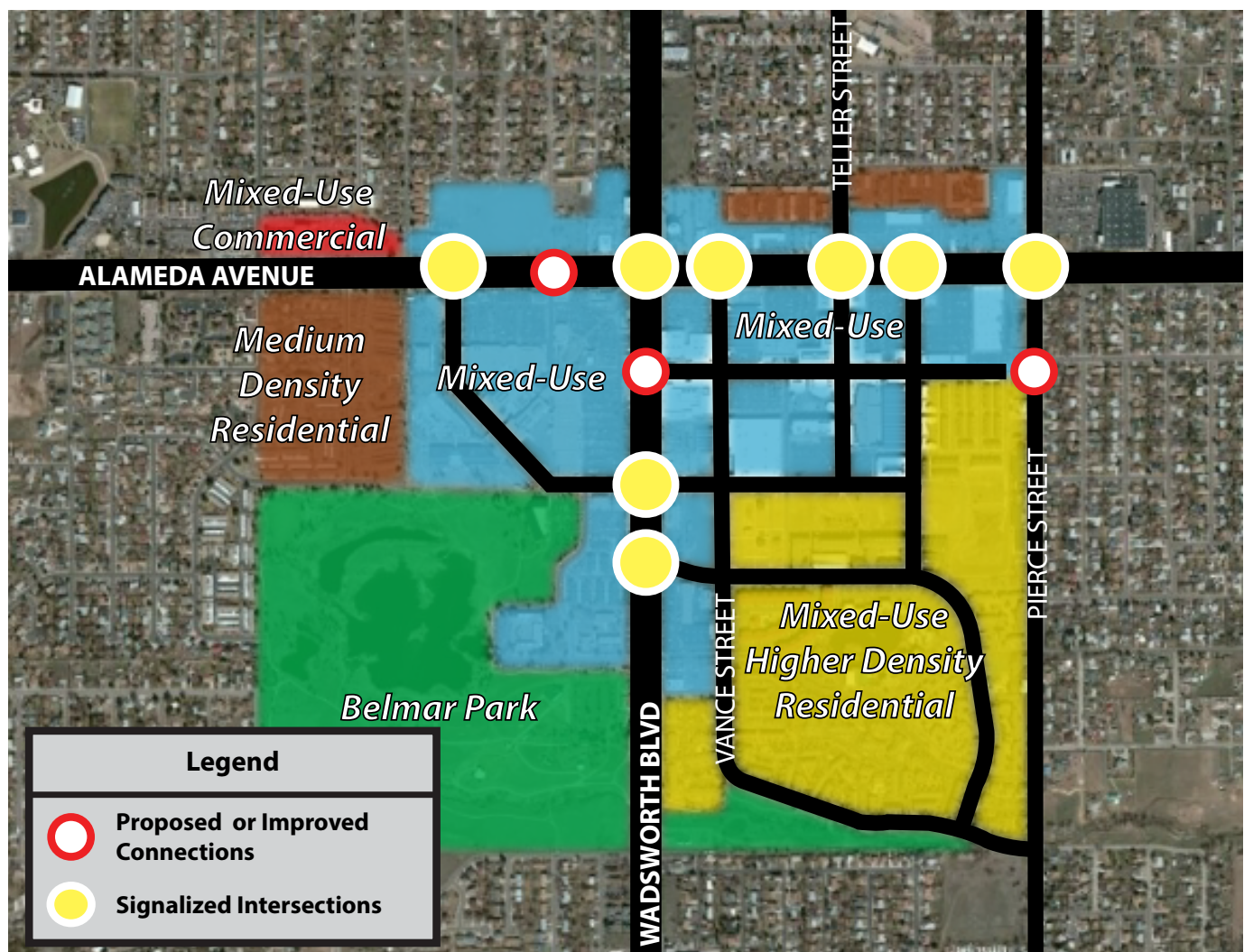


**GOAL I-GA9**

**Facilitate the continued development and transition of the Downtown Lakewood Growth Area into the premier mixed-use, entertainment, City government, and cultural destination for the City.**

**ACTION STEPS**

- Promote and market Downtown Lakewood as a premier retail, employment, residential, and visitor destination.
- Evaluate opportunities to enhance the City's art and cultural venues in downtown.
- Identify tools and marketing efforts to attract additional private cultural facilities and events to downtown.
- Develop tools to encourage projects within the area to maximize the densities and building heights allowed by zoning.

**Map 5-I****Downtown Lakewood Growth Area Land Use Map**



## GOAL I-GA10

**Work with downtown property owners and other partners to increase the regional profile of the area and increase employment and residential opportunities.**

### ACTION STEPS

- Continue to partner with Belmar ownership, through the architectural control committee and marketing efforts, to ensure that the project continues to develop as the premier mixed-use center of downtown.
- Evaluate, with Lakewood City Commons ownership, the long-term mixed-use redevelopment possibilities for the City Commons center.
- Identify and implement, with the Alameda Gateway Business Improvement District, improvements to the Alameda Avenue and Wadsworth Boulevard streetscapes within downtown.



## GOAL I-GA11

**Increase pedestrian friendliness and enhance the multi-modal transportation options in Downtown Lakewood Growth Area.**

### ACTION STEPS

- Continue to implement, and annually evaluate, the recommendations of the *Downtown Lakewood Connectivity and Urban Design Plan*.
- In conjunction with the Alameda Gateway BID, evaluate how the recommendations of the *Downtown Lakewood Connectivity and Urban Design Plan* can be funded, and pursue those funding options.
- Evaluate funding opportunities to implement improved pedestrian connections across Wadsworth Boulevard as part of Wadsworth improvement projects in the downtown area.

### Downtown Lakewood Connectivity and Urban Design Plan

The *Downtown Lakewood Connectivity and Urban Design Plan* reflects efforts to enhance the rapidly growing downtown area with increased and safer pedestrian and bicycle connectivity; stronger transit connections to the West Rail Line and enhanced transit stops; and improved wayfinding elements such as downtown identification signage, private signage recommendations and urban design elements. All of these elements are intended to unify Downtown and strengthen the area identity.

The Plan includes a number of short-term and long-term improvements to roadways and adjacent sidewalks, street intersections, landscaping in the public right-of-way, bicycle and pedestrian paths, transit service, and area identity.

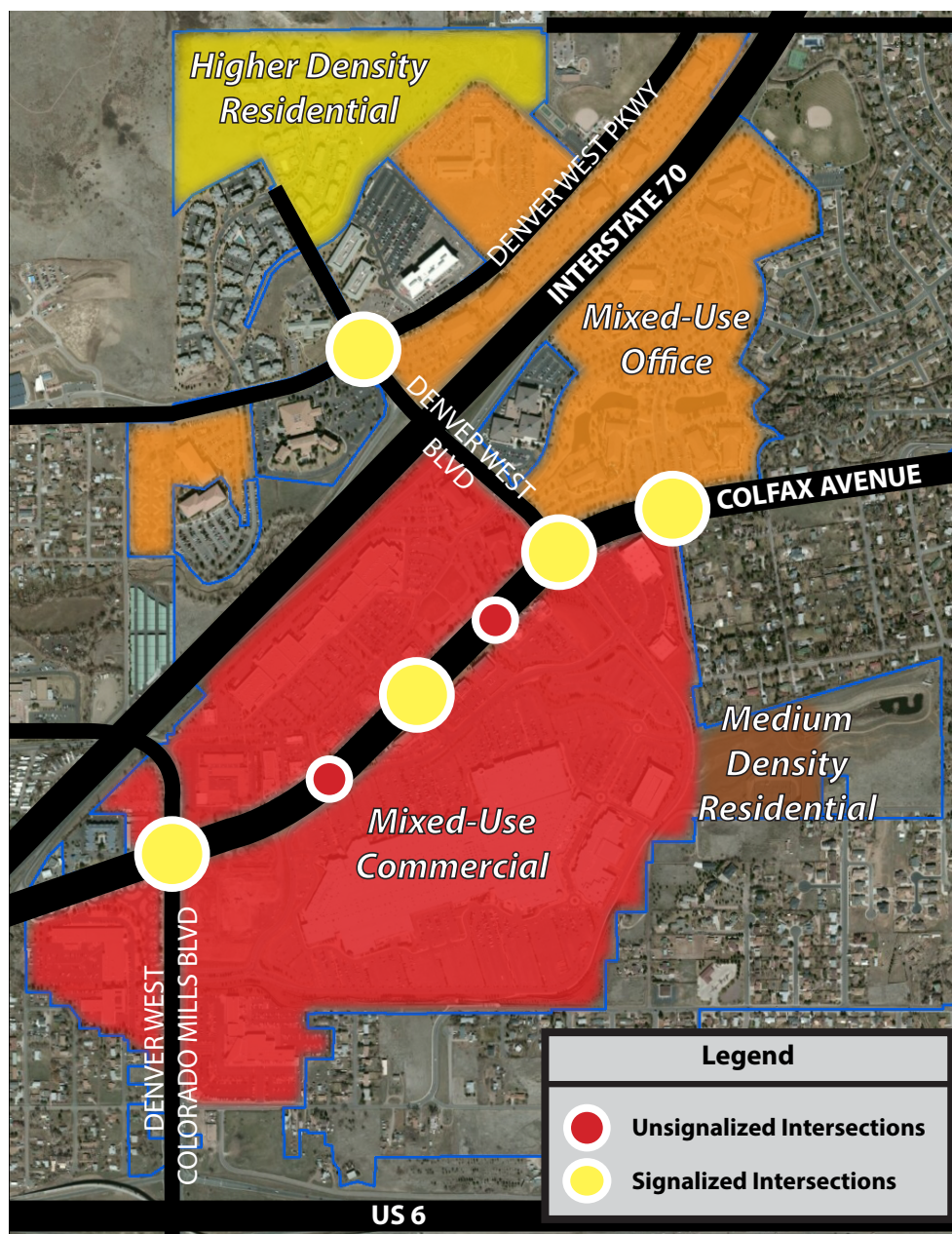




# Denver West Growth Area

The Denver West area contains a significant amount of retail and office development, as well as a growing residential population. Retail in the growth area includes Colorado Mills, with 1.1 million square feet of space and Denver West Village, with over 300,000 square feet of space. The Denver West Office Park contains nearly two dozen office buildings, with 1.25 million square feet of space. Although not in Lakewood, the National Renewable Energy Laboratory facility employs more than 1,700 people just to the northwest of the growth area. More than 500 residential units are also located within the growth area.

Opportunities for additional retail, office, and residential development exist within the area. Several vacant parcels are available for growth. Over the long term, older office building sites may be suitable for redevelopment. Additional residential development can be accommodated on property located east of the Colorado Mills project.



**Map 5-m**      **Denver West Growth Area Land Use Map**



## GOAL I-GA12

**Maintain the Denver West area as a primary regional retail and office location.**

### ACTION STEPS

- Implement a marketing campaign to encourage additional retail and entertainment uses to cluster around the Colorado Mills and Denver West Village anchors.
- Develop tools to encourage property owners to upgrade older office buildings to ensure that they meet the functional and technology needs of current and future tenants.
- Develop a plan to attract new employers to the business park through new development on vacant parcels and through infill of surface parking lots as economic conditions allow.
- Identify opportunities for gateways, way-finding, common signage elements, and branding and marketing for the City and Denver West along Colfax Avenue, Interstate 70, and US 6.
- Evaluate opportunities to connect the Denver West area to the Red Rocks West Rail Station through public transportation or private shuttle options.



## GOAL I-GA13

**Increase residential options in the Denver West growth area.**

### ACTION STEPS

- Market the area for development of additional residential communities to support employers and retailers.
- Evaluate tools to encourage the development of mixed-income housing in the area to allow workers of all types to reside close to employment opportunities.



# Jewell & Wadsworth Growth Area

The Jewell Avenue and Wadsworth Boulevard growth area is comprised of three corners of mixed-use zoned and commercially developed properties. The fourth corner includes a small office complex within the City boundary, however most of the southeast portion of the growth area is currently located in unincorporated Jefferson County and is zoned for commercial and residential development. The existing commercial centers within the growth area were originally developed between the late 1960s and early 1980s. These centers are generally lacking landscaping and are showing signs of age in appearance.

The portion of the growth area within the city is zoned for higher commercial densities than what exist today, as well as office and multifamily residential opportunities. Redevelopment within the growth area is encouraged to include a mix of uses and increased densities. Improved site access and internal circulation is also encouraged to create a street grid where appropriate.

## GOAL I-GA14

**Transform the Jewell Avenue and Wadsworth Boulevard area into a mixed-use activity center for southeast Lakewood.**

### ACTION STEPS

- In conjunction with area property owners, develop a new Jewell and Wadsworth specific area plan identifying potential street networks, land uses, and residential and employment densities for the parcels located in Lakewood and unincorporated Jefferson County.
- Evaluate potential tools, including a business improvement district, to assist with economic development and infrastructure improvements within the Growth Area.
- Determine the feasibility of creating a reinvestment area to help finance infrastructure and public amenities as part of any redevelopment.

## GOAL I-GA15

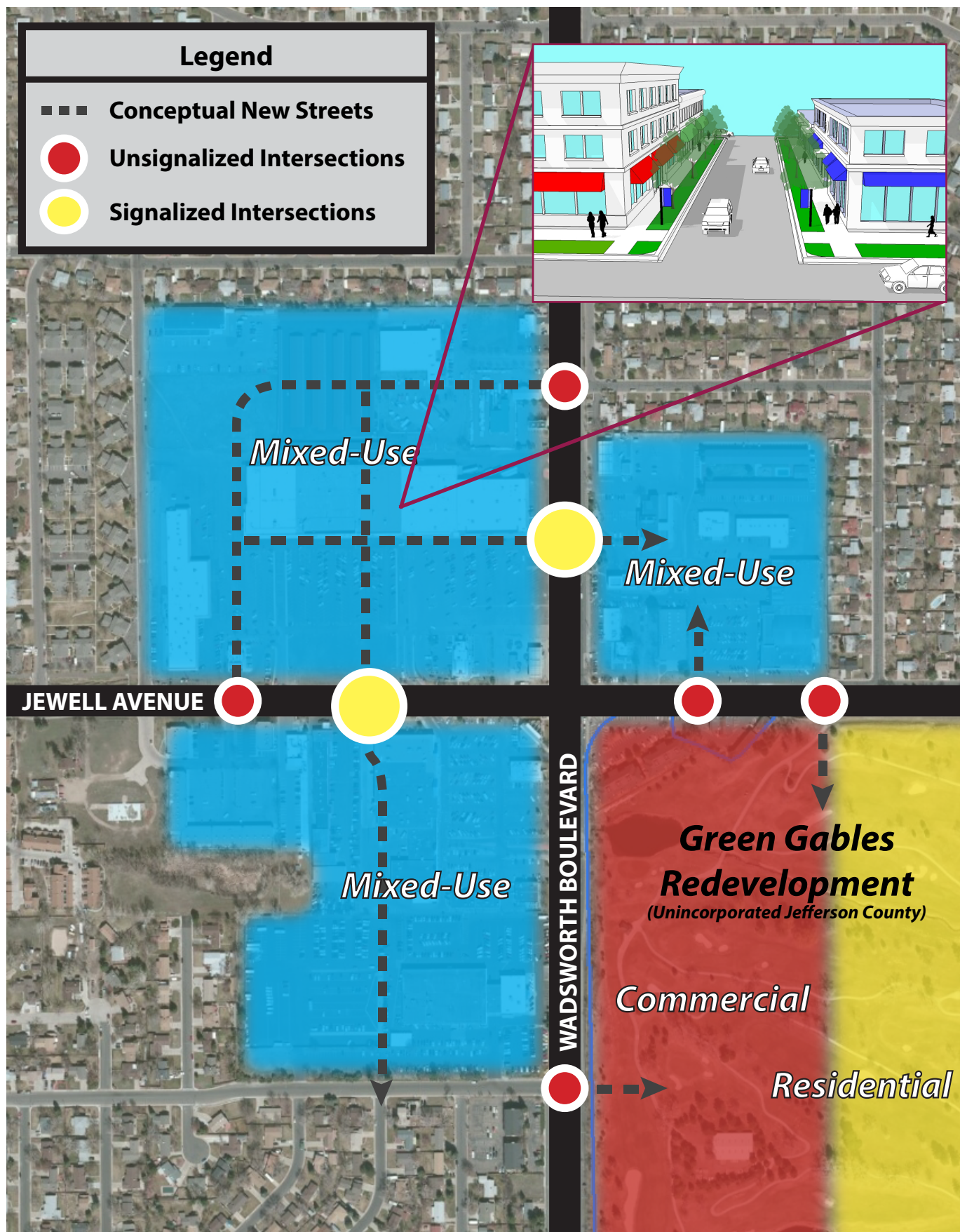
**Improve the multi-modal access to the Jewell Avenue and Wadsworth Boulevard growth area.**

### ACTION STEPS

- Evaluate opportunities to connect the Jewell and Wadsworth area to existing neighborhoods via pedestrian paths and bicycle trails.
- Evaluate improvements to multi-modal access across Wadsworth Boulevard and to improve pedestrian and bicycle safety.
- Evaluate opportunities to improve the pedestrian experience along Jewell Avenue and Wadsworth Boulevard through the installation of detached sidewalks, tree lawns, and amenity zones.







Map 5-n

Jewell &amp; Wadsworth Growth Area Land Use Map



# Rooney Valley Growth Area

The Rooney Valley growth area currently lies within three jurisdictions, with approximately two-thirds of the area within Lakewood, one-third in the Town of Morrison, and peripheral areas in unincorporated Jefferson County. Single-family residential units are the primary land use at this time, with large amounts of vacant land still available for future development.

The area has Planned Development (PD) zoning that will allow for a mix of uses in the future. Retail areas are likely to develop near the C 470 and Alameda Parkway interchange. A mix of residential types will likely occur throughout the area. Additionally, some office and support uses will likely be developed adjacent to the C 470 highway between Alameda Parkway and Morrison Road.

## GOAL I-GA16

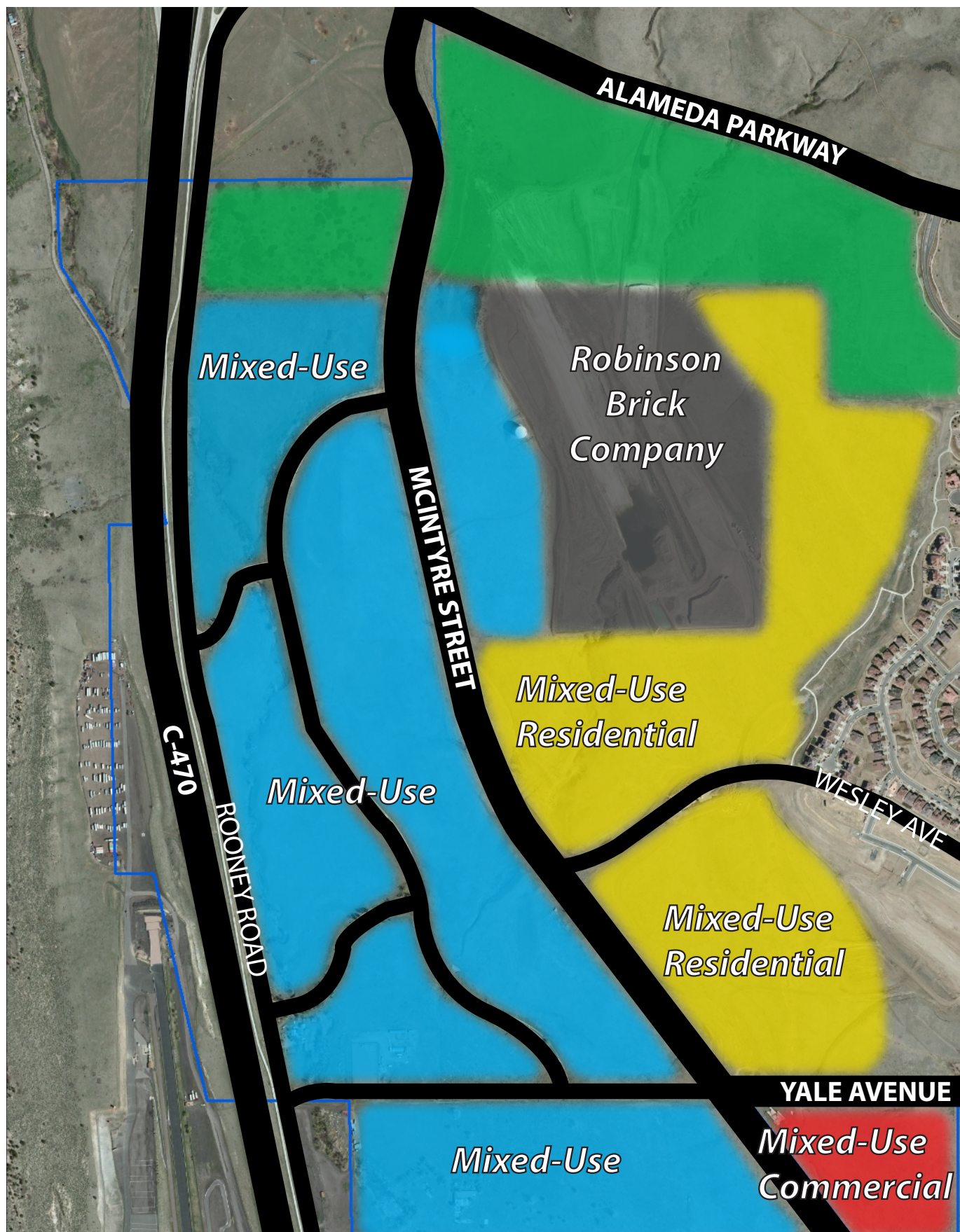
**Provide additional residential and new mixed-use opportunities in the Rooney Valley Growth Area.**

### ACTION STEPS

- a. In partnership with the Town of Morrison, Jefferson County, and property owners, evaluate and update the Rooney Valley Master Plan to ensure that the area has a proper mix of retail, office, and mixed-use developments, and a range of residential products.
- b. Continue to work with the Town of Morrison and the Rooney Valley Joint Project Review Committee to implement, and revise as necessary, the intergovernmental agreement.







Map 5-o

Rooney Valley Growth Area Land Use Map



# Mission Trace & Academy Park Growth Area

The Mission Trace and Academy Park growth area consists primarily of retail and office development on the north and south sides of US 285 (Hampden Avenue). The Mission Trace area north of US 285 was generally developed in the 1970s and 1980s. The area has no undeveloped land. The Academy Park area south of Hampden Avenue is still being developed with a mix of office, restaurant and hospitality uses.

In the long term, redevelopment opportunities exist within the Mission Trace area. The Academy Park area will continue to develop with a mix of non-residential and residential uses. The southwest corner of the Hampden Avenue and Wadsworth Boulevard interchange is currently located within unincorporated Jefferson County and is a prime location for mixed-use development.



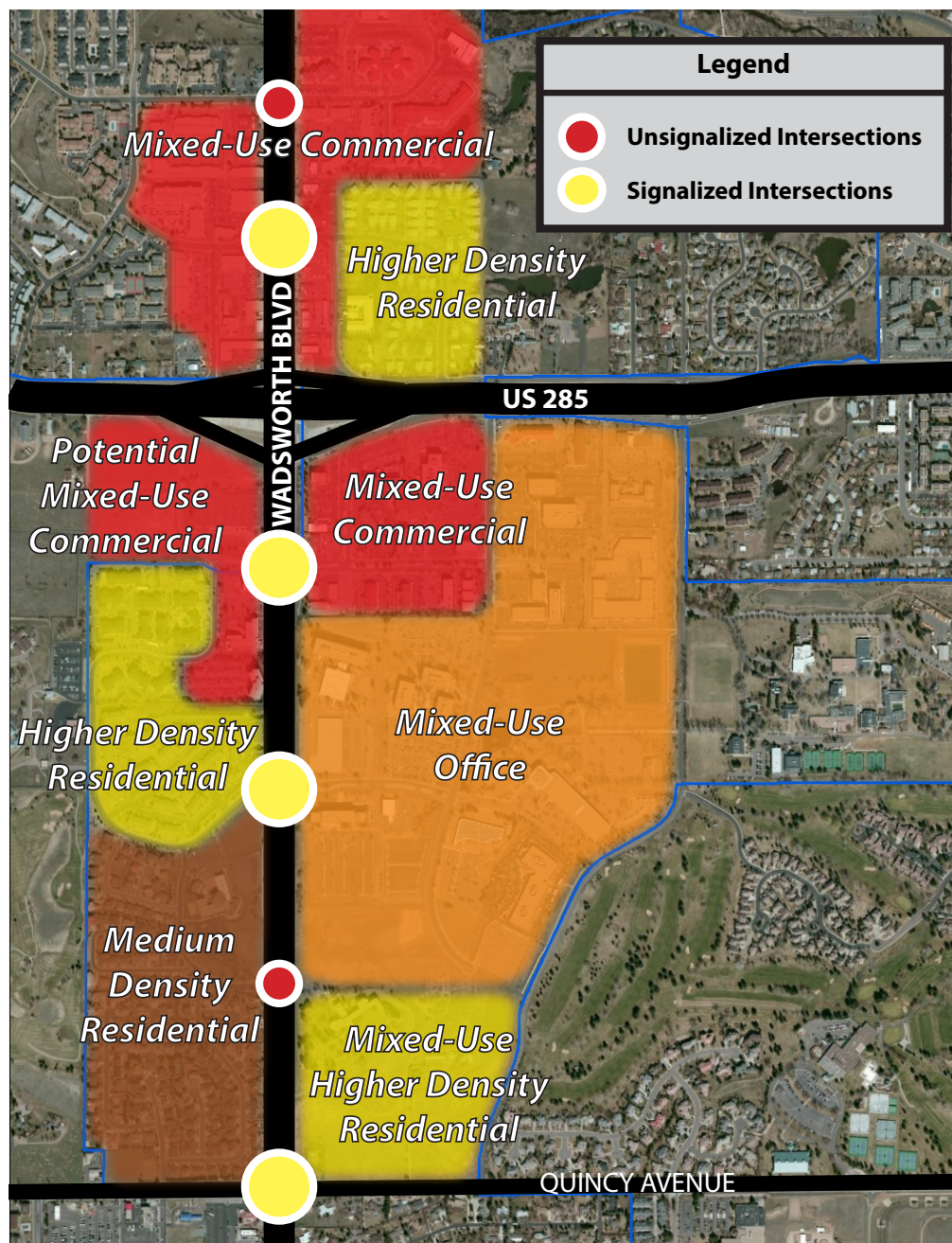
## GOAL I-GA17

**Enhance the existing office and regional retail environment of the growth area.**

### ACTION STEPS

- a. Evaluate, with area property owners, opportunities to improve multi-modal access into the retail areas from Wadsworth Boulevard north of US 285.
- b. Evaluate opportunities to create cross-access between properties throughout the growth area.
- c. Evaluate and implement improved multi-modal crossings of Wadsworth Boulevard at signalized intersections in the area.
- d. Evaluate the potential for annexation and mixed-use development of the under-utilized sites in the southwest quadrant of the Wadsworth Boulevard and US 285 interchange.
- e. Develop tools to encourage property owners to upgrade older office buildings to ensure that they meet the functional and technology needs of current and future tenants.
- f. Evaluate opportunities to increase office occupancy in Academy Park.
- g. Market opportunities for additional residential uses to both the Mission Trace and Academy Park areas.
- h. Evaluate potential tools, including a business improvement district, to assist with economic development and infrastructure improvements within the Growth Area.
- i. Develop a branding and marketing campaign for the area.





Map 5-p

Mission Trace/Academy Park  
Growth Area Land Use Map**GOAL I-GA18**

**Create a southern gateway to the City along Wadsworth Boulevard at US 285.**

**ACTION STEPS**

- Develop a gateway plan for the growth area, that includes opportunities to unify the area through way-finding, landscaping, and common signage elements.
- Evaluate opportunities to install landscaping and design elements within medians located north and south of US 285.

# COMMUNITY ACTIVITY AREAS

## Overview

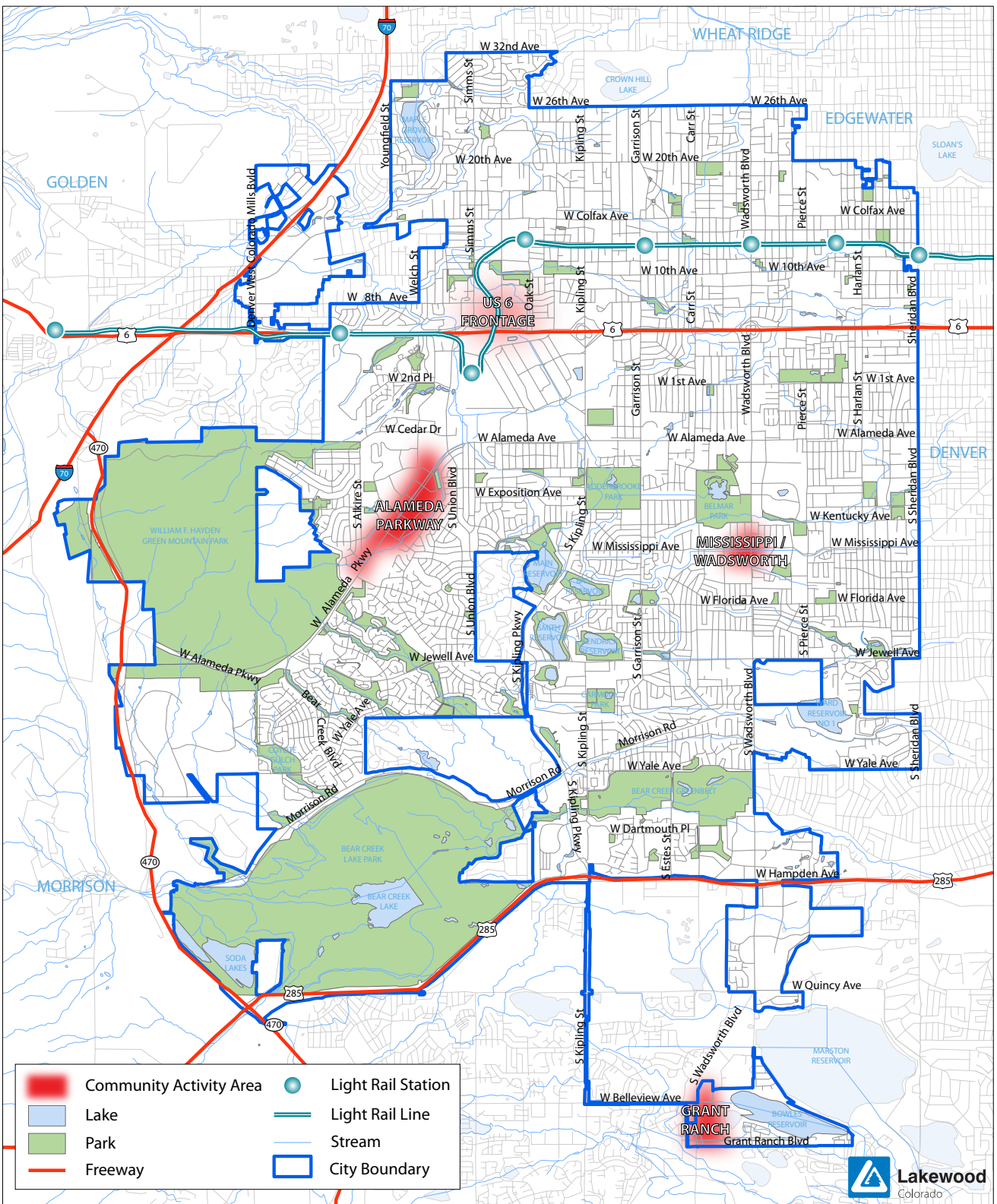
Community Activity Areas are areas that have been planned and zoned primarily for higher density employment and retail growth. However, residential development may be allowed by zoning in certain activity areas. These areas are found at several locations throughout Lakewood. Within the northern portion of the city, a Community Activity Area can be found along the north frontage road of US 6 between Kipling and Simms Streets. Community Activity Areas are also located along Alameda Parkway west of Union Boulevard, at Mississippi Avenue and Wadsworth Boulevard and along Wadsworth Boulevard in the Grant Ranch area of the city.

Community Activity Areas are intended to accommodate additional employment and retail opportunities on a smaller scale than those found in Growth Areas. These areas are generally intended for pedestrian-friendly projects with single or multi-story buildings. Developments within these areas generally consist of one or more buildings including 100,000 to 500,000 square feet of leasable retail and office space. This type of development typically serves a relatively large trade area within a 10-20 minute drive time.

Community Activity Areas are located along major streets that provide adequate vehicular access, but are also close to residential neighborhoods with bike and pedestrian access.

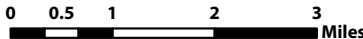
The designated Community Activity Areas are indicated on Map 5-q on the following page.





## Map 5-q

## Community Activity Areas



# US 6 Frontage Road Community Activity Area

The US 6 Frontage Road community activity area generally serves the northern and central Lakewood area, and Denver area residents with a mix of larger retailers and office buildings. The activity area includes several furniture stores, as well as motor vehicle dealerships. The area is suburban in nature and is generally accessed by automobile.

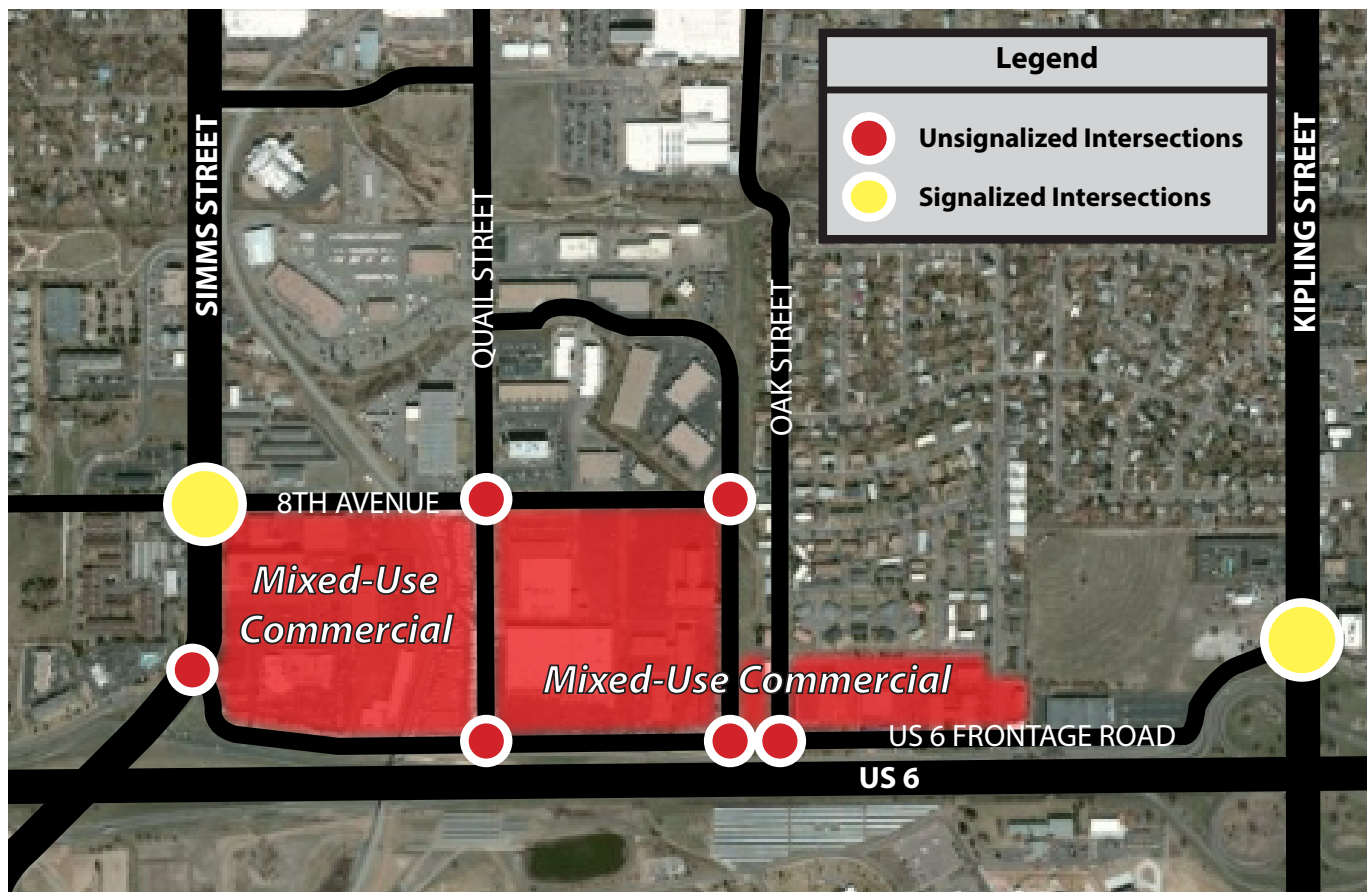
The activity center is intended to remain a primarily commercial and office area serving the surrounding neighborhoods and a portion of the western Denver region. Limited residential development may be appropriate in the long term, particularly near the proposed Routt Street crossing of US 6.

## GOAL I-CA1

**Maintain the area as a viable community-scale shopping and office district.**

### ACTION STEPS

- Develop tools to encourage property owners to rehabilitate aging buildings and sites, and redevelop older or inefficient commercial and office buildings in the activity area.
- Evaluate opportunities to increase the mix of uses in the area.
- Develop a set of unified design standards for the area.



**Map 5-r**

**US 6 Frontage Road Community Activity Area Land Use Map**



# Alameda Parkway Community Activity Area

The Alameda Parkway community activity area is located within, and generally serves, the Green Mountain and Foothills neighborhoods. The activity area includes two grocery-anchored shopping centers, as well as several smaller retail centers and free-standing businesses. The area is suburban in nature and is generally accessed by automobile.

The activity center is intended to remain a primarily commercial and office area serving the surrounding neighborhoods. However, some additional residential development may be appropriate in the long term.

## GOAL I-CA2

**Maintain the area as a viable community-scale shopping and service district.**

### ACTION STEPS

- Develop tools to encourage property owners to rehabilitate aging buildings and sites, and redevelop older or inefficient commercial and office buildings in the activity area.
- Develop a plan and design guidelines for the area with a focus on improved multi-modal access and landscape treatments.



**Map 5-s**

**Alameda Parkway Community Activity Area Land Use Map**

# Grant Ranch Community Activity Area

The Grant Ranch community activity center is located in the far southeastern portion of the city and primarily serves residents in unincorporated Jefferson County and Denver. The activity center includes a number of large format retailers, as well as restaurants and a small amount of office space. This center is suburban in nature and generally accessed by automobile.

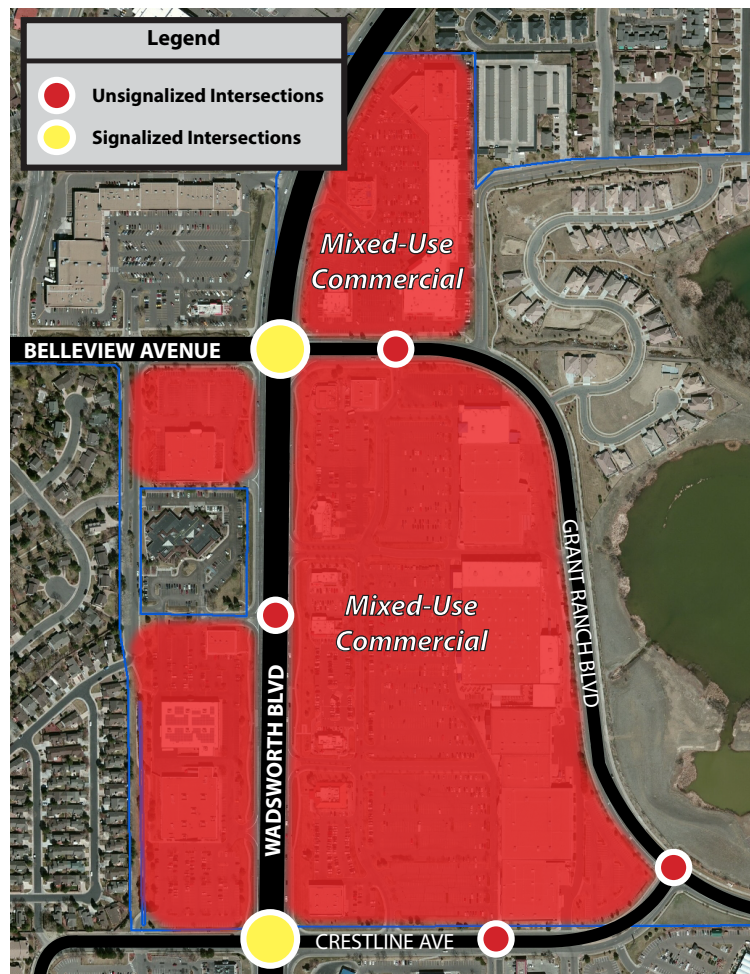
The activity center is intended to remain a primarily commercial area serving the surrounding neighborhoods, while contributing to the city's long-term financial health.

## GOAL I-CA3

**Maintain the area as a community and regional retail draw for Lakewood residents, and residents of Denver and unincorporated Jefferson County.**

### ACTION STEPS

- Track the economic performance of the area to ensure that it maintains the role of a sales tax base generator for Lakewood.
- Develop tools to encourage property owners to make building and site improvements as necessary to maintain the viability of the area.



**Map 5-t** Grant Ranch  
Community Activity Area Land Use Map



# Mississippi & Wadsworth Community Activity Area

The Mississippi Avenue and Wadsworth Boulevard community activity center provides services to the central Lakewood neighborhoods. The area consists of smaller retailers and a number of office buildings. The area is suburban in nature, but has adequate pedestrian, bicycle, and automobile access to adjacent neighborhoods.

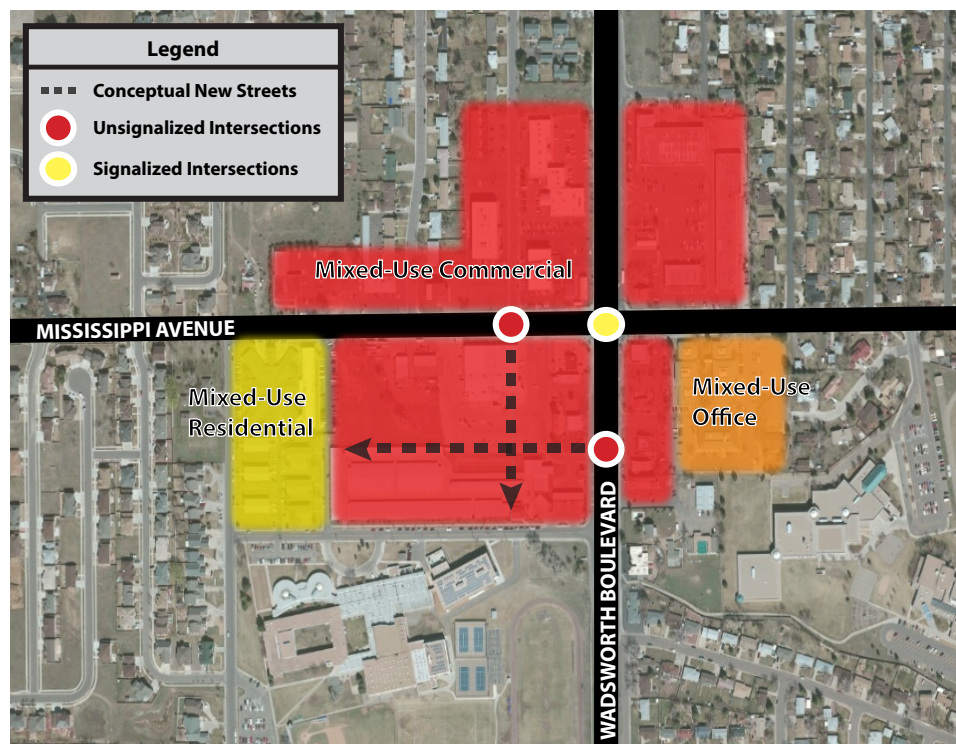
The activity center is intended to remain a primarily retail and office area, but opportunities for residential development are available with redevelopment. Long-term redevelopment opportunities exist within the entire activity center area.

## GOAL I-CA4

**Encourage the transition of the area into a more cohesive community-scale retail and office center.**

### ACTION STEPS

- Work with property owners to create a redevelopment plan for the area that includes an evaluation of the need for additional office space and opportunities to reinvest in the buildings that currently exist.
- Evaluate opportunities to create additional bicycle and pedestrian connections to surrounding neighborhoods with any redevelopment within the area.
- Develop tools to encourage the development of mixed-use and residential projects within the area.



**Map 5-u**

**Mississippi & Wadsworth  
Community Activity Area Land Use Map**

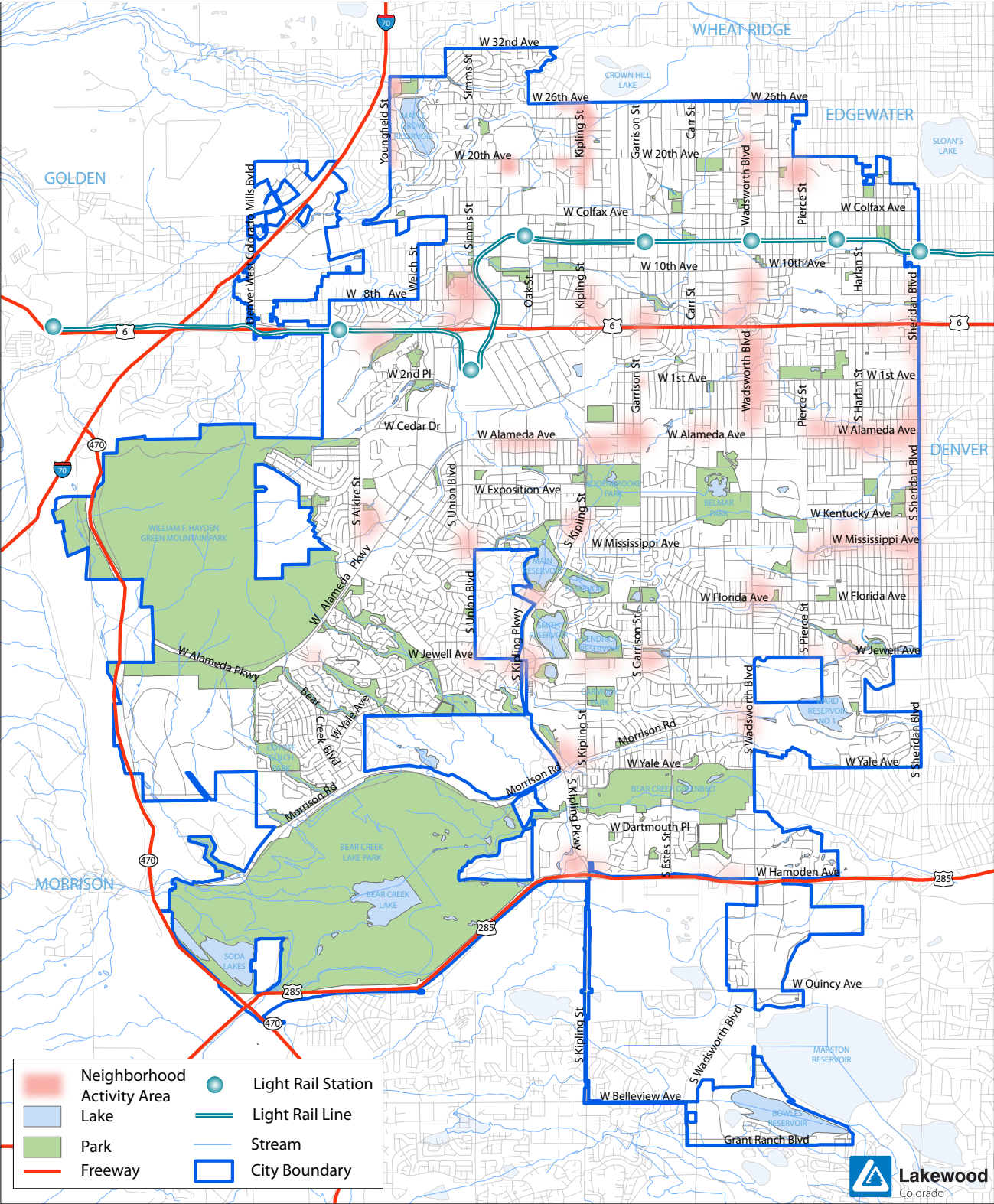
# NEIGHBORHOOD ACTIVITY AREAS

## Overview

Neighborhood Activity Areas are small-scale commercial and mixed-use areas primarily found within or in close proximity to residential neighborhoods. These areas are generally accessible to pedestrians and bicyclists, as well as motor vehicles. These areas provide for the convenience and service needs of the surrounding neighborhoods and typically contain uses such as small office, dental, non-emergency medical facilities, small restaurants, and small fueling stations.

Neighborhood Activity Areas are found throughout the city, as identified on Map 5-v on the following page.





## GOAL I-NA1

**Encourage the evolution of Neighborhood Activity Areas into small-scale mixed-use centers.**

### ACTION STEPS

- a. Develop tools to encourage property owners to rehabilitate aging buildings and sites, redevelop older or inefficient buildings into mixed-use structures, and to encourage residential unit construction on upper floors of new development within these activity areas to provide for a local feel.
- b. Develop policies that encourage the development of outdoor seating areas as part of any restaurant within the activity areas.
- c. Evaluate the economic condition of the activity areas and prioritize those that are in greater need for reinvestment.





**GOAL I-NA2****Create community focal points within Neighborhood Activity Areas.****ACTION STEPS**

- a. Develop tools to encourage the inclusion of community gathering places as part of reinvestment or redevelopment within the activity areas.
- b. Work with property owners to evaluate the feasibility of developing community gardens or small private parks within the activity areas.
- c. Evaluate opportunities to increase pedestrian and bicycle access from adjacent neighborhoods to the activity areas.

# FUTURE ACTIVITY AREAS

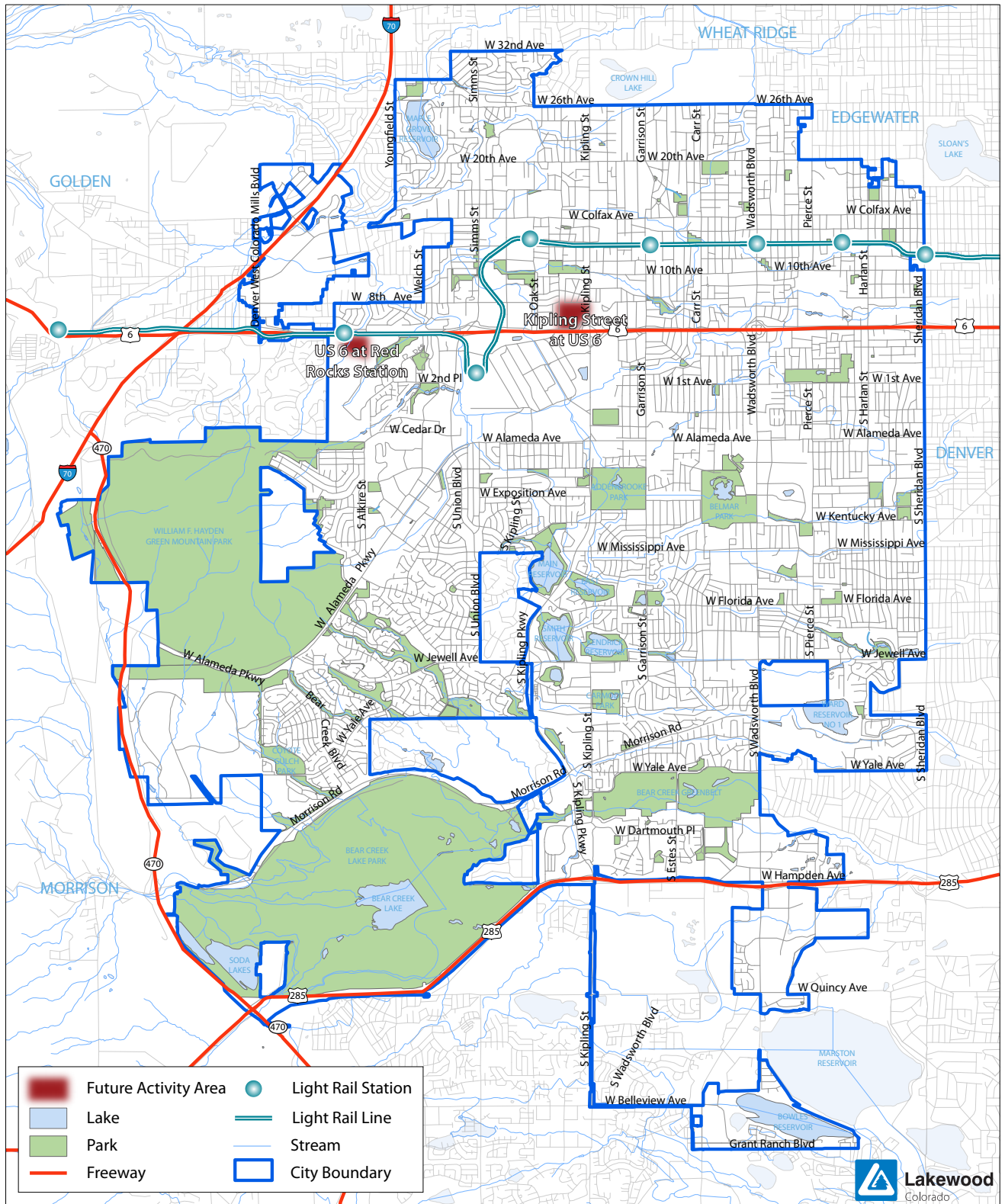
## Overview

As the city's population and employment base continues to grow, additional activity areas will need to be created. These future activity areas may be either community or neighborhood scale. Locations for two additional community activity areas have been identified. These areas are currently vacant, but are prime locations due to highway and light rail access. However, both have constraints that must be addressed as well.

Additional activity areas should be evaluated in the future as the city continues to grow. While the two activity areas currently identified are mixed-use and commercially zoned, other future areas may need to be rezoned. Such rezonings will need careful consideration and neighborhood input.

The designated Future Activity Areas are indicated on Map 5-w on the following page.





# US 6 at Red Rocks Station Activity Area

The US 6 at Red Rocks site is located adjacent to the US 6 Frontage Road and the entrance to the Red Rocks Community College, across the frontage road from the Red Rocks West Rail Line station platform. This site is vacant and owned by the Colorado State Board for Community Colleges and Occupational Education. It is currently part of the Red Rocks Community College campus, but will likely not be utilized for educational facilities.

The site is appropriate for a community-scale activity center, as it is in close proximity to the college, neighborhoods and the light rail station. However, to provide access to many neighborhoods located north of US 6, a pedestrian bridge over the highway would need to be constructed. Additionally, grading would need to occur, as the site is located on a hillside above the frontage road.



## GOAL I-FA1

**Create a new Community Activity Area at the Red Rocks Station.**

### ACTION STEPS

- Initiate a planning effort with the Red Rocks Community College administration to determine the most appropriate uses for the site and create a master plan for the property.
- Determine funding opportunities for the construction of a pedestrian bridge across US 6 to provide access to the activity center and the rail station from the neighborhoods north of US 6.



# Kipling Street at US 6 Activity Area

The Kipling Street at US 6 area was previously the site of a drive-in theater and a state office building. The two uses have since been removed from the site and it is now vacant. There are also several parcels surrounding the site that include hotels, restaurants, and a big box retailer that could also be incorporated into development within the area.

The site is appropriate for a community-scale activity center, as it is in close proximity to US 6, neighborhoods, and the Denver Federal Center. However, access to the site is currently complicated by a frontage road that is located too close to the Kipling Street and US 6 interchange. For the site to develop to its full capacity, the frontage road will need to be relocated through the property.

## GOAL I-FA2

**Create a new Community Activity Area at US 6 and Kipling Street.**

### ACTION STEPS

- Evaluate realignment options for the frontage road and site access.
- Evaluate the current zoning to determine if mixed-use development on the site, with appropriate buffering of the existing residential neighborhoods located to the north and west of the site, is more appropriate.



# Additional Future Activity Areas

As the city continues to see population and employment increases, additional activity areas may be necessary to provide the desired services within close proximity to existing and new residents and employees. The City should be proactive in determining when and where new activity areas should be developed, and the scale of the new activity areas.

## GOAL I-FA3

**Determine when and where new activity centers should be developed to meet the needs of the city.**

### ACTION STEPS

- a. Annually evaluate the need for additional activity areas, based on population and employment changes and work with property owners and adjacent neighborhoods to determine the scale of, and the appropriate zoning for, a particular activity area, when it is determined necessary.
- b. Develop a plan for new activity areas that include neighborhood or community gathering spaces, and can be adequately accessed by all modes of transportation.



# INDUSTRIAL AREAS

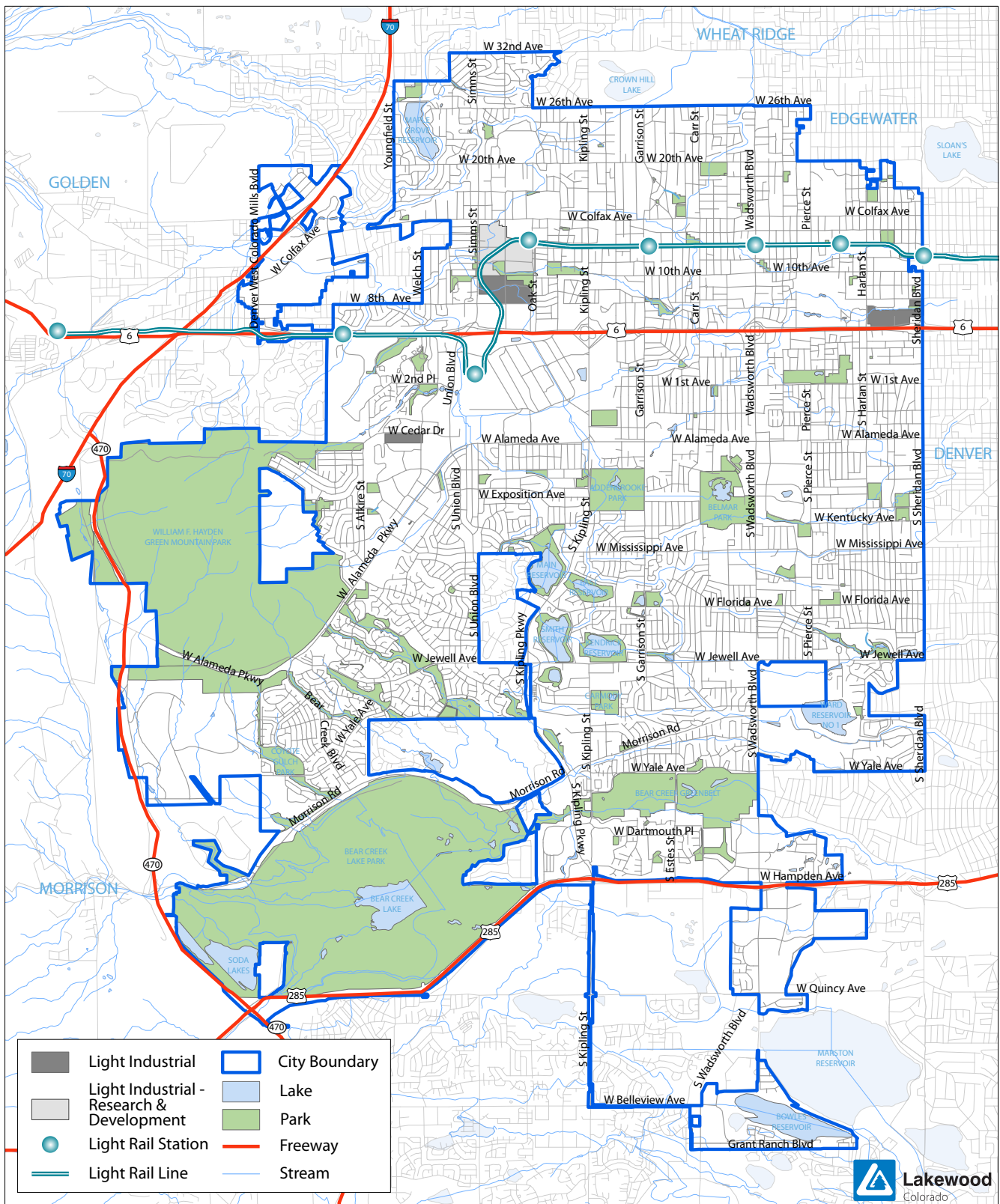
## Overview

Although the City's Zoning Ordinance allows for light industrial and light manufacturing uses in a number of districts, it is important to maintain the last few remaining industrially zoned properties for more intense manufacturing and processing uses.

Only two large areas of Light Industrial (LI) zoned properties exist in the city. They are located in the northwest quadrant of the US 6 and Sheridan Boulevard interchange, and east of Simms Street between approximately 8th and 10th avenues. These areas currently include a mix of light industrial, retail, service, and office uses.

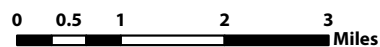
The existing light industrial areas are indicated on Map 5-x on the following page.





### Map 5-x

## Industrial Areas





**GOAL I-IA1**

**Maintain the existing light industrially zoned properties to allow for a diversity of employers within the City.**

**ACTION STEPS**

- a. Evaluate the redevelopment possibilities of properties located within the light industrial areas, but that do not contain industrial uses. Explore opportunities to relocate the nonindustrial businesses to more appropriate locations within the city.
- b. Facilitate the development of light industrial and flex office space on vacant industrially zoned land and reinvestment of existing older industrial and flex space properties.
- c. Evaluate the negative impacts of any request to rezone light industrial areas to another zone district.



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